



City of Visalia

City Council

Meeting Agenda - Final

Visalia City Council
707 W. Acequia
Visalia, CA 93291

Mayor Steve Nelsen
Vice Mayor Brian Poochigian
Council Member Greg Collins
Council Member Brett Taylor
Council Member Liz Wynn

Monday, March 7, 2022

7:00 PM

Visalia City Council Chambers

Work Session Start Time: 5:30 PM

ROLL CALL

CALL TO ORDER WORK SESSION - 5:30 PM

PUBLIC COMMENTS

Citizens are now invited to comment on issues within the jurisdiction of the Visalia City Council. The Council asks that you keep your comments concise and positive. Creative criticism, presented with appropriate courtesy, is welcome.

Each speaker will be allowed three minutes and a timer will notify you when your time is expired. Please begin your comments by stating and spelling your name and providing your city of residence.

WORK SESSION AND ACTION ITEMS (as described)

1. Continue review of select FY 2022/23 and 2023/24 Capital Improvement Program (CIP) Budgets and future capital plans.

[22-0011](#)

ADJOURN TO CLOSED SESSION - 6:00 PM (Or, immediately following Work Session)

CALL TO ORDER REGULAR SESSION - 7:00 PM

PLEDGE OF ALLEGIANCE

INVOCATION

ITEMS OF INTEREST

PUBLIC COMMENTS

This is the time for citizens to comment on subject matters that are not on the agenda and within the jurisdiction of the Visalia City Council. The Council asks that you keep your comments brief and positive. Creative criticism, presented with appropriate courtesy, is welcome.

This is also the time for citizens to request an item from the Consent Calendar be pulled for discussion purposes. Public comments related to all pulled Consent Calendar items and Regular or Public Hearing items listed on the agenda will be heard at the time that item is discussed or when the Public Hearing is opened.

Each speaker will be allowed three minutes, and a timer will notify you when your time is expired. Please begin your comments by stating and spelling your name and providing your city of residence.

CONSENT CALENDAR

Consent Calendar items are considered routine and will be enacted in one motion. There will be no separate discussion of these matters unless a request is made and then the item will be removed from the Consent Calendar to be discussed and voted upon by a separate motion.

1. Authorization to read ordinances by title only. [22-0097](#)
2. Authorization to direct staff to appropriate American Rescue Plan Act (ARPA) funding from the Federal Transit Administration (FTA) to the Transit Operations budget (4551), and to approve allocation requests up to \$1,827,167. [22-0044](#)
3. Authorize staff to accept and appropriate funding for fire department training projects that will be reimbursed through the California Firefighter Joint Apprenticeship Committee (Cal-JAC) account in the amount of \$94,770. [22-0069](#)
4. Authorize the City Manager to award a sole source three-year contract, with the option to renew for two additional years, for city-wide web hosting and connectivity for fleet Geo Positioning System (GPS) to Cal/Amp for an amount not to exceed \$35,910 per year. [22-0073](#)
5. Second Reading and adoption of Ordinance No. 2022-01, approving Conditional Zoning Agreement No. 2021-11, an amendment submitted by CenterPoint Integrated Solutions to Conditional Zoning Agreement No. 2003-01, removing provisions that prohibit the establishment of a "stand-alone" used automobile sales use, and adding development standards for "stand-alone" used automobile sales uses for the Visalia Auto Plaza located within the C-S (Service Commercial) Zone. The Visalia Auto Plaza is located west of North Neeley Street, north of West Camp Drive, and south of the Mill Creek Ditch (APN: 081-020-085). [22-0087](#)

REGULAR ITEMS AND PUBLIC HEARINGS

Comments related to regular Items and Public Hearing Items are limited to three minutes per speaker, for a maximum of 30 minutes per item. The Mayor may reasonably limit or extend the public comment period to preserve the Council's interest in conducting efficient, orderly meetings.

1. Request for an indefinite continuance of the following item: Public Hearing to consider an appeal of the Planning Commission's approval of Conditional Use Permit No. 2021-28, a request by Caldwell-Mooney Partners II, L.P. to allow a medical use within a portion of an existing 10,200 square foot multi-tenant commercial building. The new medical clinic will be approximately 6,526 square feet in area and will be located within the C-R (Regional Commercial) zone district. The project is located at 3221 South Mooney Boulevard (APN: 121-100-054). Resolution No. 2022-04 required. [22-0096](#)

CLOSED SESSION REPORT

ADJOURNMENT

Upcoming Visalia City Council Meetings:

Monday, March 21, 2022 - Regular Meeting @ 7:00 PM - 707 W. Acequia

Thursday, March 24, 2022 - Special Joint Meeting @ 4:30 PM - 915 S. Mooney Blvd

Monday, April 4, 2022 - Regular Meeting @ 7:00 PM - 707 W. Acequia



City of Visalia

Staff Report

Visalia City Council
707 W. Acequia
Visalia, CA 93291

File #: 22-0011

Agenda Date: 3/7/2022

Agenda #: 1.

Agenda Item Wording:

Continue review of select FY 2022/23 and 2023/24 Capital Improvement Program (CIP) Budgets and future capital plans.

Deadline for Action: 3/7/2022

Submitting Department: Finance and Technology Services

Contact Name and Phone Number:

Melody Murch, (559)713-4379, melody.murch@visalia.city <<mailto:melody.murch@visalia.city>>

Renee Nagel, (559)713-4375, renee.nagel@visalia.city <<mailto:renee.nagel@visalia.city>>

Department Recommendation:

That the City Council receive the fourth presentation of the City's Capital Improvement Program (CIP) report and provide direction as appropriate. The Council is scheduled to act upon the CIP budget in June in conjunction with the operating budget. No formal action is required or requested at this time.

Background Discussion:

The Review Process- This is Council's fourth opportunity to review a portion of the City's proposed Capital Plan. Portions of the proposed 2022/23 and 2023/24 capital budget are being brought to Council for review at each scheduled meeting during the months of February-March 2022. This allows Council to conduct their review of the proposed CIP budget in manageable groupings.

The Capital Improvement Program presents the two fiscal years 2022/23 and 2023/24 for adoption and appropriation. *Four additional years are shown for planning purposes only.* This allows Council to plan future projects and consider the near-term impacts of those decisions. However, only the first two years are proposed for appropriation and deserve greater scrutiny. Amounts included in the final four years will not be appropriated in the 2-year budget.

Staff has prepared a Power Point presentation to review the proposed Capital Programs of the various funds discussed below. CIP Project Managers will be available to address Council's questions on proposed projects. To assist Council in reviewing the proposed CIP projects, staff recommends the following:

- Staff will present a summary of proposed project appropriations and provide Council with the opportunity to request further information on individual projects.
- It is recommended that Council review and either tentatively approve or direct changes to the attached proposed Capital Improvement Program (CIP) budgets for the selected funds.
- No formal action is required at this time. At Council's direction, staff will return with additional

information or changes to the CIP at a following meeting, until all projects have been reviewed, and Council is satisfied with the proposed capital budget for all funds.

- A final Capital Improvement Program document will be prepared, after the review process is completed, and included for adoption with the City's Operating Budget in June 2022.

The CIP includes all projects and equipment purchases costing \$10,000 or more. When possible, Capital Projects are separated from the operating budget, using separate capital funds. This allows a clear view of capital activities and prevents operating budgets from being artificially inflated by those same capital activities.

Funds for Review- The seven funds to be reviewed today include one impact fee and six special revenue funds.

- Impact Fee funds hold revenue from fees imposed upon new development to pay for the proportionate costs of providing public infrastructure to serve that new development. These funds can only be used for the purchase or construction of new or expanded infrastructure related to the growth from development.
- Special Revenue Funds collect money which must be used for a specific purpose. These funds provide accountability and transparency to tax and rate payers, requiring funding to be spent for an intended purpose.

Transportation Funding: The Funds included in this report represent the majority of revenues available to the City for the construction and maintenance of public roadways within the City of Visalia. Also included is the Measure R Regional Fund which provides funding for projects of regional significance. The only transportation capital funding excluded from this item is the Street Maintenance funding contained in the Measure N budget, which is subject to an additional review by the Measure N committee and will be brought to Council for review and discussion on May 2, 2022.

The Citizen's Advisory Committee annual surveys have shown consistently over the years that maintenance of City roads, traffic signs, and signals is a high priority for the citizens of Visalia, second only to public safety. The Pavement Management System (PMS) is used to determine the annual amount of funding required to maintain Visalia's streets. New construction of City infrastructure, including roads, is driven by development, and the funds to be discussed tonight include projects which have been based upon projections of growth and development in the City of Visalia. Construction of new, and improvement of existing street infrastructure supports and encourages new development.

The Funds to be discussed in today's meeting include those listed in Table 1: Capital Budget Funds for Review March 7, 2022, below.

Table 1: Capital Budget Funds for Review March 7, 2022

Fund Name	Fund #	Proposed 2022-23	Proposed 2023-24
Gas Tax	111	2,679,800	2,615,000
SB1-RMRA	113	4,100,000	3,300,000
Measure R Local	131	2,419,800	3,892,000
Measure R Regional	133	-	-
Transportation Impact Fees	241	6,979,200	5,239,300
Landscape & Lighting	273	252,000	766,000
State Transportation	281	275,000	1,875,000
Total Projects		\$ 16,705,800	\$ 17,687,300

Gas Tax, Fund 111 (CIP detail attached): This fund receives the local portion of state excise taxes on gasoline and diesel fuel which is allocated through the state Highway Users Tax Account. The highway user's tax revenues come from special taxes (sales and excise taxes-sections 2103-2107.5) on the sale of transportation fuels which are levied by the State. As a result of the Prop 42* swap which began in 2011, the funds formerly received by the City for the Traffic Congestion Relief fund are included in the Gas Tax fund. These amounts are allocated by the State on a combination of factors including population, registered vehicles, and per capita amounts.

***Prop 42:** Requires the State to fully replace the amount cities would have received under the old sales tax structure with their share of the new excise tax which was included in the legislation for Prop 42, with the goal of making the measure "revenue neutral".

Gas Tax funds may be used for construction, improvements, and maintenance of streets and roads. To maximize funding for street maintenance, 70% of the projects proposed in the Gas Tax fund are for maintenance projects throughout the City. Roughly 28% of the Gas Tax fund is currently planned for New Construction, Improvements, and projects to be completed in conjunction with New Construction, with the remainder to be used for the replacement of equipment used by the City Street Maintenance division. However, most projects for construction and improvement of city streets are

programmed from Measure R, Transportation Impact Fees and other State and Federal grant programs which are typically housed in the State Transportation Fund (281).

Maintenance projects proposed to receive appropriations in the 2-year portion of the budget include thin skin patching, crack sealing, reclamite, dig outs, minor asphalt overlays, pothole patching, and cape seal. The 2-year capital budget also proposes funding for the replacement of specialized streets equipment including a large compaction roller, crack filler, and oil distributor. The equipment being replaced ranges in age from 22 to 28 years old and has reached the end of its useful life.

The currently proposed capital budget follows the pattern of past practice for street maintenance. Of the 27 proposed projects in this fund, 23 are proposed for funding in the 2-year portion of the budget. For more detail on Gas Tax projects, please see the fund summary included in Attachment A.

SB1-Road Maintenance and Rehabilitation Act (RMRA), Fund 113 (CIP detail attached): This fund, which was established under Section 2032(H)(2) of the Streets and Highway Code with the adoption of SB1, addresses maintenance on local streets and roads. The funding may be spent on road maintenance and rehabilitation, safety projects, railroad grade separations, complete street components, and traffic control devices. Projects eligible for SB1 funding are required to be identified in detail and passed through local government channels with binding resolutions before applying for the state funds. As was intended, this funding has allowed the City to begin addressing deferred maintenance on its transportation infrastructure. This fund has a single annual project in each of the 6 years of the capital plan which is for pavement rehabilitation. This project is discussed below.

- Pavement Rehabilitation - SB1 (\$3.3million to 4.1 million annually): Rehabilitation of various streets throughout the City of Visalia based on priority list established by the Pavement Management System (PMS). Street rehabilitation is done to improve the ride quality, add structural strength, and extend the life expectancy of a street. This project is a longer lasting and important maintenance project for City streets that will be coordinated with the Public Works Department through analysis from Pavement Management System.

For more detail on the SB1-RMRA fund resources and projects, please see the fund summary included in Attachment A.

Measure R Local, Fund 131 (CIP detail attached): This fund receives monies from the local portion of the Measure R 1/2 cent Transportation Sales Tax approved by voters of Tulare County in November of 2006. Local agencies collectively receive 1/3 of all Measure R revenues for local projects. These funds may be expended on transportation projects as determined by each local jurisdiction. The local fund is proposing 34 projects, with 25 requesting appropriations in the 2-year portion of the budget. These projects include various traffic control and roadway improvement projects. Three of the larger projects included in the 2-year portion of the capital budget are the installation of two new traffic signals and a new railroad crossing in the northwest quadrant of the city which are discussed below:

- Northwest Quadrant At-Grade Railroad Crossing (\$4,850,500*, 2023-2027): This project will complete a study to determine the optimal location for a new at-grade crossing (possibly Linwood Street, Chinowth Street, or Preston/Roeben Street) and the abandonment of an existing crossing. Construction will include the installation of new pavement, curb, gutter, sidewalk, traffic signal, railroad warning devices, signage and striping at recommended location and will upgrade the existing railroad tracks. Also included are public outreach, abandonment of the existing RR crossing and moderate updates to two adjacent intersections north and south of the new crossing. Development in the northwest quadrant of the city has created the need for an additional railroad crossing to improve circulation. Funding in FY23/25 will provide for preliminary engineering and environmental, plans, specifications, and estimates (PS&E), Right of Way and Permitting. FY26/27 funding is for construction. This project is multi funded and the project total of \$4.8m will be paid from \$2.4m Measure R Local (131) and \$2.4m Transportation Impact Fees (241). ****Appropriations of \$686,800 are in the 2-year portion of the budget (\$200,000 from Measure R Local, and \$486,800 from Transportation Impact Fees). The project balance of \$4,163,700 is in the planning years.***
- Mooney and Ferguson Traffic Signal (\$800,000, 2022-2024): For the installation of a traffic signal at Mooney and Ferguson. The current all-way stop intersection meets the traffic signalization criteria defined by the California Manual on Uniform Control Devices (MUTCD) as determined through the data collected with the Annual Traffic Data Collection Contract. This project includes design and construction. The multi-funded project total of \$800,000 will be paid from \$550,000 of Local Transportation Development Funds (281) and \$250,000 Measure R Local (131).
- Shirk and Ferguson Traffic Signal (\$800,000, 2023-2025): For the installation of a traffic signal at Shirk and Ferguson. The current all-way stop intersection meets the traffic signalization criteria defined by the California Manual on Uniform Control Devices (MUTCD) as determined through the data collected with the Annual Traffic Data Collection Contract. This project includes design and construction. The multi-funded project total of \$800,000 will be paid from \$275,000 Local Transportation Development Funds (281) and \$525,000 Measure R Local (131).

For more detail on the remaining projects proposed for Measure R Local funding, please see the fund summary included in Attachment A.

Measure R Regional, Fund 133 (CIP detail attached): This fund receives monies from the regional portion of the Measure R 1/2 cent Transportation Sales Tax approved by voters of Tulare County in November of 2006. Regional projects in Tulare County collectively receive 1/2 of all Measure R revenues. The Tulare County Transportation Authority (TCTA) administers the Measure R

expenditure plan which restricts how this money can be spent. This fund has been included in today's discussion based upon the funding proposed for the Widening of Riggan from Shirk to Akers in the third year of the capital plan (2024-25). This planned project would be multi-funded with the Transportation Impact Fee fund, where it is included in the third and fourth years of the capital plan (2024-2026).

A detailed description of this project is included in the Measure R Regional fund summary included in Attachment A.

Transportation Impact Fees (TIF), Fund 241 (CIP detail attached): This fund is derived from developer fees collected at the time of building permit issuance or, if the developer requests, at the time of occupancy. Funds can be used only for construction of new street improvements and expansion of transportation facilities related to growth as included in the Circulation Element of the City's General Plan.

TIF Program: When the City approves a project for development, it is committing to provide the necessary community-wide public improvements, including transportation infrastructure, to support that development. When setting the price of development impact fees, staff projects the development need based upon the current General Plan and its elements, namely the Circulation Element. All available sources of funding for required infrastructure are considered when TIF fees are reviewed. Impact fees are the funding of last resort and are set equal to the transportation plan's revenue shortfall. The intent of impact fees is to ensure the necessary funding for the proportionate costs associated with new development and its impact on City infrastructure.

The City's TIF Fund continues to project a large deficit in the 6-year capital plan and the needs of this funds clearly outweigh the resources. Projects are planned and prioritized based upon projected development and must be chosen based upon the need to support new development as it occurs. Over the next two years, staff will continue to evaluate the projects included in the planning years and recommendations for which projects to move forward will be brought back in the next capital plan. The widening of Riggan Avenue from Kelsey to Shirk is the largest project in this fund and is discussed below.

- ***Riggan Widening - Kelsey to Shirk (\$10,975,000*, 2022-23):*** This project will widen Riggan Avenue between Kelsey Street and Shirk Road from an undivided two-lane road to a four-lane divided road with a median. The project will include installation of a drainage swale, street lights and interconnect conduit. Once the EDA grant is received and bonding from TCAG is successful, this project will move forward. Widening of Riggan Ave will improve mobility of traffic for the industrial park and the surrounding businesses while reducing congestion. ****The project total of \$10,975,000 is multi-funded from \$2,200,000 of prior year Measure R Regional, \$4,775,000 of Transportation Impact (\$2,400,000 from prior year), and \$4,000,000 of EDA Grant, which is contingent upon award.***

The TIF fund has 26 projects in the 6-year plan with 16 of those projects proposed for appropriations

in the 2-year portion of the budget. For more detail on proposed TIF projects, please see the fund summary included in Attachment A.

Landscape and Lighting, Fund 273 (CIP detail attached): This fund receives property tax assessments on property owned within each Landscape and Lighting (L&L) District to be used for maintenance of common areas, street maintenance and landscaping. Funds may be used only within each individual L&L District for costs included in the engineer's report prepared at the time of the creation of the L&L District.

The 6-Year Capital Plan shows only the portion of L&L District assessments that is set aside for street maintenance. The portion of this fund which provides for landscape and lighting will be brought to Council in June along with the operating budget. In 2005, newly formed L&L Districts began to include street maintenance in their engineer's reports and assessment amounts. There are now 61 L&L districts that include street maintenance. Based upon the street maintenance schedule included in the individual L&L's engineer's reports, reclaimer, crack seal, overlay, and cape seal projects are proposed during the 2-year portion of the CIP budget.

All L&L's which include street maintenance also include an automatic inflation of the annual assessment amount. The cash balance of each L&L is evaluated annually and automatic increases only take effect if they are found to be necessary. Street maintenance projects for individual L&L's will be grouped together annually and combined with other like projects for bidding to achieve any available economies of scale. For more detail on Landscape and Lighting street maintenance projects, please see the fund summary included in Attachment A.

State Transportation, Fund 281 (CIP detail attached): This fund is derived from 1/4 cent of statewide sales tax collected and returned to each County in compliance with the Local Transportation Development (LTD) Act. These funds are administered by the Tulare County Association of Governments (TCAG) and the first priority for use of the funds is public transit (buses). After transit needs are met, the remaining monies, as well as various discretionary revenues, may be used for road and street purposes, including bike/pedestrian facilities. Transportation related grants awarded to the City of Visalia are also housed in this fund. These monies are to be used only for approved transportation projects in compliance with the requirements of their individual funding sources. This fund has 9 proposed projects in the 6-Year Capital Plan with 2 projects proposed to receive appropriations in the 2-year budget. These projects include the installation of a traffic signal at Mooney and Ferguson, and construction of a portion of the Greenway Trail, from Mill Creek to St. John's. The new traffic signal is multi-funded with Measure R Local funds and is discussed in that section above.

For more detail on State Transportation Fund projects, please see the fund summary included in Attachment A.

Fiscal Impact:

The City's Capital Improvement Plan details a six-year plan for the expenditure of funds and the completion of projects (see Attachment A). Information for the years 2024/25 through 2027/28 is provided for informational and planning purposes and will be revised, updated, and adopted in 2-year increments.

The attached spreadsheets summarize each fund and provide a detailed list of the proposed projects. The fund title and number are followed by a brief explanation of the revenue source and general purpose of the fund. Each fund summary shows projections of the beginning cash, operating revenue (or capital contribution from operating revenue), operating expenditures, proposed CIP expenditures, and the projected ending cash balance for each of the six years.

Prior Council Action:

The following is a list of Capital Funds reviewed at prior Council Sessions:

Table 2: Capital Funds Reviewed at Prior Council Sessions

FUND	Key	DESCRIPTION
2/7/22		
406	4060	Animal Services
411	4040	Airport
452	4520	Transit Capital

FUND	Key	DESCRIPTION
2/22/22		
103	2330	Corporation Yard Impact Fee
231	2490	Wastewater Trunkline
431	4330	Wastewater
441	4410	Solid Waste
501	5010	Fleet Maintenance

FUND	Key	DESCRIPTION
2/28/22		
132	2420	Measure R Bike/Trail
211	2440	Recreation Facilities
221	2450	Storm Sewer
222	2460	Storm Sewer Deficiency
224	2480	Ground Water Recharge
251	2530	Waterway Maintenance
261	2540	Waterway Construction
481	4880	Storm Sewer Maintenance

Other: Table 3 is a summary of the remaining capital funds to be reviewed by Council during the month of March 2022, prior to their adoption:

Table 3: Capital Funds to be reviewed by Council in future Council Sessions

FUND	Key	DESCRIPTION
3/14/22		
012	3820	Police Unclaimed Funds
502	5020	Vehicle Replacement
503	5030	Measure T Police Vehicle Replace
504	5040	Measure T Fire Vehicle Replace
511	5110	Information Services

FUND	Key	DESCRIPTION
3/21/22		
001	1010	General Fund
106	2360	Fire Impact Fee
413	4130	Convention Center
611	6010	Parking In-Lieu

04/18/2022*		
311	6110	CDBG

05/02/2022*		
141	1410	Measure N

** These funds will be reviewed separately from the Capital Budget process according to the requirements of the individual funding, but will be included in the final budget document for informational purposes.*

Alternatives: None Recommended.

Recommended Motion (and Alternative Motions if expected):

No motion is required or requested at this time.

Environmental Assessment Status: N/A

CEQA Review: N/A

Attachments:

Attachment A: Summary of proposed Capital Projects by Fund

Attachment B: Directory of Map Points and Maps

Attachment A: Summary of Proposed Capital Projects by Fund

Gas Tax - 111 (2370) 2022/23 - 2027/28 Capital Improvement Program

This fund includes monies derived from Sections 2103, 2105, 2106, 2107 and 2107.5 of the Streets and Highways Code. These revenues come from special taxes (excise taxes) on the sale of transportation fuels which are levied in addition to the state sales tax. Allocations are generally distributed on the basis of population and registered vehicles. This fund also receives money from the State Highway Fund that is distributed to each region based on population and road miles. Funds are to be used only for construction, improvements and maintenance of streets and roads.

Cash Balance Summary

	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Beginning Cash Balance	(780,100)	(94,300)	736,100	663,500	681,100	1,750,400
Gas Tax Apportionment (Sections 2105, 2106, 2107, 2107.5)	2,730,100	2,784,700	2,840,400	2,897,200	2,955,100	3,014,200
Gas Tax Swap (Prop 42)	1,258,200	1,283,400	1,309,100	1,335,300	1,362,000	1,389,200
Interest Earnings	10,000	10,000	3,300	3,400	8,700	13,900
Services Provided/Allocations	(632,700)	(632,700)	(645,400)	(658,300)	(671,500)	(684,900)
Capital Improvements - New Construction, Improvements and Administration	(300,000)	(480,000)	(1,555,000)	(1,425,000)	(560,000)	(560,000)
Capital Improvements - Street Maintenance	(2,025,000)	(2,135,000)	(2,025,000)	(2,135,000)	(2,025,000)	(2,135,000)
Capital Improvements - Vehicle and Equipment Replacements or Additions	(354,800)	0	0	0	0	0
Total Resources Available for Future Projects	(94,300)	736,100	663,500	681,100	1,750,400	2,787,800

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Projects for New Construction, Improvements and Administration:											
1	Roadway Preliminary Engineering: This project is for preliminary engineering and design work needed for potential roadway capital projects. This project is also needed for ancillary engineering work such as improvement standards and standard specifications and provides an operating budget for preliminary work on future projects involving grants and routine items that may not be captured in a specific capital improvement project.	Frank Senteno	CP9699	--	N/A	75,000	75,000	75,000	75,000	75,000	75,000
2	Developer Reimbursements: This CIP is for reimbursement of public assets constructed in the public right of way. The continued and ongoing maintenance of the improvements completed under this CIP is budgetted in a separate City CIP.	Paul Bernal	CP9207	--	N/A	75,000	75,000	75,000	75,000	75,000	75,000
3	Traffic Signal Modification at Chinowth and Whitendale: Modify existing traffic signal at the intersection of Chinowth Street and Whitendale Avenue to provide protected left turns in all directions and incorporated intelligent transportation equipment. Traffic delays have increased significantly since the traffic signal was constructed in 2002. The protected left turns will provide for more efficient movement of vehicles, reduce delays, and minimize traffic collisions.	Eric Bons	PWCP03	--	C9	50,000	125,000				

Attachment A: Summary of Proposed Capital Projects by Fund

Gas Tax - 111 (2370) - Continued 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Projects for New Construction, Improvements and Administration: (continued)											
4	Traffic Counts, Survey, and Supplemental Services: To assist staff with increasing traffic monitoring, maintaining traffic speed zones, and the analysis of various intersections to help relieve vehicle congestion. City growth increases the number of vehicles, bikes, or pedestrians traveling through intersections. Yearly traffic counts will be performed to ensure appropriate traffic control. In addition, these traffic counts help provide information for economic development and traffic signal coordination.	Leslie Blair	CP8101	--	N/A	50,000	55,000	55,000	55,000	60,000	60,000
5	Bike Plan Implementation: This annual project will implement the most recent Bike Plan, which includes striping bike lanes and adding signs to improve safety for bicyclists, as well as improvement to traffic flow for vehicles and bikes. These improvements support recommendations from the City's adopted Active Transportation Plan (ATP). Where applicable with other roadway CIP's, these funds will be used to implement bike lanes.	Wyndi Ferguson	CP9923	--	N/A	50,000	50,000	50,000	50,000	50,000	50,000
6	Upgrade Traffic Signal Controllers: This project will upgrade obsolete traffic signal controllers with the newer Model 2070 which possesses updated processing and capacity for integration into the traffic management system. Originally, 127 controllers required replacement with 43 controllers remaining to be upgraded. Funding will upgrade the remaining controllers over the course of two years starting FY 24-25.	Eric Bons	CP0285	--	N/A		100,000	130,000			
7	Main-Center One-Way Conversion: Main Street & Center Avenue one-way conversion between Santa Fe Street and Ben Maddox Way forming existing two-way streets to a one-way couplet. Project includes design engineering and construction based upon the Traffic Study for Main/Center One-Way Conversion project completed in 2020 (CP0296). Multi funded project total of \$2.15m from \$410k Measure R Local (131) and \$1.74m Gas Tax (111)	Leslie Blair	PWCP77	--	B13			870,000	870,000		
8	Pedestrian Ramps & Intersection Improvements: Reconstruct 16 pedestrian ramps and replace intersection pavement at 8 identified intersections in the downtown area over a course of three years beginning FY2025-26. Design will be performed in the interim. Project will improve ADA accessibility and reduce maintenance at each ramp location. Ramp Locations are at Center Avenue at Hall, Park, Dudley, Turner, and Conyer streets; Willis Street at School, Oak, and Willow Avenues. The pedestrians ramps will be made ADA compliant and may require intersection reconstruction. This project will remedy these ramps that currently have substandard "half-round" pipe drainage systems.	Casey Chu	CP0404	--	B14			300,000	300,000	300,000	300,000

Attachment A: Summary of Proposed Capital Projects by Fund

Gas Tax - 111 (2370) - Continued 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Projects for Street Maintenance throughout the City:											
9	Thin Skin Patching: Patch portions of pavement, temporarily, with an asphalt cold mix. Thin skin patching is an effective and fast method of preventative maintenance performed on smaller portions of pavement that have deteriorated due to deferred maintenance. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leaving the City open to liability claims.	Wyndi Ferguson	CP9520	--	N/A	700,000	700,000	700,000	700,000	700,000	700,000
10	Crack Seal: Placement of rubber type based product in cracked pavement. Crack sealing is performed to reduce water penetration, thereby helping to maintain the structural strength of the asphalt and limit degradation, reduce potholes and prevent the cracks from getting larger. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leaving the City open to liability claims.	Wyndi Ferguson	CP9496	--	N/A	350,000	350,000	350,000	350,000	350,000	350,000
11	Reclamite: Cost-effective pavement maintenance of various City streets when performed routinely (within 5-7 years) after initial proper street installation which results in cost savings for pavement life span compared to other methods of pavement maintenance or replacement. Provides rejuvenation of asphalt oils to drying and worn pavement surfaces. This project implements recommendations from the Pavement Management System to perform large cape seal and reclamite projects to assist in the catch-up of street maintenance from prior years. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leaving the City open to liability claims.	Wyndi Ferguson	CP9510	--	N/A	200,000	200,000	200,000	200,000	200,000	200,000
12	Annual Striping Contract: The existing pavement markings need to be repainted to be in compliance with the California Manual of Uniform Traffic Control Devices (MUTCD). This repainting of the pavement markings will provide delineation of travel lanes, turn lanes, crosswalks, curb markings and other pavement markings on the roadways throughout the City.	Wyndi Ferguson	CP9226	--	N/A	160,000	160,000	160,000	160,000	160,000	160,000

Attachment A: Summary of Proposed Capital Projects by Fund

Gas Tax - 111 (2370) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
13	Dig Outs: Remove and replace isolated, deteriorated pavement on various city streets. This will improve the ride quality of city streets, reduce pothole patching, delay the need of costly reconstruction and possibly reduce claims from damage to vehicles. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leaving the City open to liability claims.	Wyndi Ferguson	CP9508	- -	N/A	100,000	100,000	100,000	100,000	100,000	100,000
14	Minor Asphalt Overlays: Placement of an asphalt overlay on local streets, citywide. This is a cost-effective way to extend the useful life of a street with asphalt in minor stages of deterioration. Provides additional structure and strength to existing asphalt, as well as a smoother and safer traveling surface. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leaving the City open to liability claims.	Wyndi Ferguson	CP9525	- -	N/A	100,000	100,000	100,000	100,000	100,000	100,000
15	Pot Hole Patching: Patch portions of pavement, temporarily, with an asphalt cold mix. Pothole patching is an effective and fast method of preventative maintenance on small portions of pavement that have deteriorated due to water penetration into the base. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leaving the City open to liability claims.	Wyndi Ferguson	CP9515	- -	N/A	100,000	100,000	100,000	100,000	100,000	100,000
16	Cape Seal: Places an emulsified seal over weathered and raveled pavement without substantial amount of cracking. This project implements recommendations from the Pavement Management System to perform large cape seal and reclamite projects to assist in the catch-up of street maintenance from prior years. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leaving the City open to liability claims.	Wyndi Ferguson	CP8007	- -	N/A	75,000	75,000	75,000	75,000	75,000	75,000
17	Pave Outs: Restore pavement that has been cut away during replacement of damaged curb and gutter, and approach ways that are not adjacent property owners responsibilities. This will improve environmental conditions and maximize use of roadway throughout the City. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leaving the City open to liability claims.	Wyndi Ferguson	CP9094	- -	N/A	75,000	75,000	75,000	75,000	75,000	75,000

Attachment A: Summary of Proposed Capital Projects by Fund

Gas Tax - 111 (2370) - Continued 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
18	Replace Traffic Signs: Replace regulatory, warning, and guide traffic signs that do not meet the reflectivity compliance based on the Manual of Uniform Traffic Control Devices (MUTCD). Includes contracting services of signs for blank purchases, fabrications, refurbishments, and recycling of old signs. This funding is only for City owned traffic sign.	Wyndi Ferguson	CP9555	--	N/A	75,000	75,000	75,000	75,000	75,000	75,000
19	Replace Street Name Blades: Annual replacement of Street Name Blades (SNB) to bring them into compliance with correct design and reflectivity, per the Manual on Uniform Traffic Control Devices (MUTCD). SNB's are a means of direction for emergency services. This will allow replacement of SNB's citywide due to reflectivity to maintain visibility for first responders to prevent slowing down response times. It will also standardize the SNB's making the manufacturing and replacing process easier for the City and subcontractors. The City has 8,000 SNB's at an average cost of \$50 each (\$400k total). Approx. 1,000 SNB's will be replaced annually.	Wyndi Ferguson	CP0406	--	N/A	40,000	40,000	40,000	40,000	40,000	40,000
20	Battery Backup Maintenance: Annual replacement of existing and new batteries to ensure traffic signals (TS) remain operational in an emergency due to electricity loss. There are 130 TS in the City, with approximately 70% of them installed with battery back up systems. Without this back up power service, traffic at intersections would be unsafe and very difficult to pass through. Each signal controller requires 4 batteries when replaced, with a lifespan of 4-6 years per battery. The remaining 90 traffic signal batteries will be replaced over the next 6 years, and continue to be replaced per the batteries useful lifespan.	Wyndi Ferguson	CP0407	--	N/A	20,000	20,000	20,000	20,000	20,000	20,000
21	Hot Patching: Allows for repairing of small areas of alligatored asphalt or potholes surrounded by generally good condition asphalt. Used for small repairs in colder weather situations. A heating device is used to heat the existing asphalt roadway (maximum 6' x 8' area) in order to introduce small amounts of new hot asphalt paving with fresh oil emulsions; blending the new with the old (existing). Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leaving the City open to liability claims.	Wyndi Ferguson	CP8351	--	N/A	20,000	20,000	20,000	20,000	20,000	20,000

Attachment A: Summary of Proposed Capital Projects by Fund

Gas Tax - 111 (2370) - Continued 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
22	Traffic Signal Maintenance: Replacement of aging traffic signal cabinets, replacement of existing in-pavement vehicle detection with video detection and other minor upgrades to existing signalized intersections. As the existing equipment fails or is damaged, replacement of existing traffic signal cabinets allows for use of newer equipment as the City's Intelligent Transportation System expands. Allows for the attachment of a battery backup system during power outages. Replacement of existing in-pavement vehicle detection with video detection allows for greater flexibility for pavement repair and modifications to the lane configurations. Per year, replace one traffic signal controller cabinet and retrofit one intersection with video detection.	Eric Bons	PWCP04	--	N/A	10,000	90,000	10,000	90,000	10,000	90,000
23	Neighborhood Traffic Calming: Install traffic calming devices when criteria is met and in accordance with the Neighborhood Traffic Management Program (NTMP). Program addresses resident's concerns regarding excessive vehicle speed and traffic volumes on neighborhood streets. The NTMP Policy outlines a process for neighborhood groups (experiencing these concerns) to work with the City to improve their quality of life. Funds will allow for the installation of 2-3 devices per each year funded. A portion of this project has been deferred due to the lack of funding.	Leslie Blair	CP8282	--	N/A		30,000		30,000		30,000
24	Maintain Pavement Management Plan: To maintain the data on street conditions (Pavement Condition Index) and sign inventories this system stores and maintains sign retroreflectivity data. Data will be used to create annual maintenance and repair programs using different methods to extend the life of the pavement and signage. City staff to gather data and inventory all roads that have been maintained or rehabbed. Consultant to re-survey the City street network in FY24/25 to acquire a representative pavement condition index for the City. Re-survey will be conducted every five to six years to recalibrate/update the Pavement Management Plan.	Chantha Chap	CP8313	--	N/A			350,000			

Attachment A: Summary of Proposed Capital Projects by Fund

Gas Tax - 111 (2370) - Continued 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Projects for Vehicle and Equipment replacements or additions:											
25	Replace Ingersol Rand Vibratory Compaction Roller to comply with CARB regulations. Roller is in compliance until December 2023. Budget Year / Unit / Age / Hours 2022-23 / 314855 / 22 / 991	Wyndi Ferguson	PWEQ06	- -	N/A	180,400					
26	Replace Crack Filler that has reached the end of its useful life. Budget Year / Unit / Year / Age / Hours 2022-23 / 314734 / 1999 / 22 / 3,769	Wyndi Ferguson	PWVH08	- -	N/A	87,200					
27	Replace Trailer Mounted Oil Distributor that has reached the end of its useful life. Budget Year / Unit / Year / Age 2022-23 / 314760 / 1993 / 28	Wyndi Ferguson	PWVH09	- -	N/A	87,200					
						2,679,800	2,615,000	3,930,000	3,560,000	2,585,000	2,695,000

Notes: Multi-funded means this is only this fund's portion of the total amount budgeted. These projects are funded from multiple sources and the full budget is shown in the "Multi-Fund 301".

The Budget Impact column shows the estimated annual impact of the project on the operating budget for maintenance once completed or purchased.

- - No Annual Maintenance Costs (or no increase over existing cost)

\$\$\$ Annual Maintenance costs is over \$25,000. These projects will be explained in detail in the project description.

\$ Annual Maintenance cost is \$5,000 or less

\$\$\$\$ Project will result in savings as described in project description

\$\$ Annual Maintenance costs is \$5,000 to \$25,000

Attachment A: Summary of Proposed Capital Projects by Fund

SB1 RMRA - 113 (2390) 2022/23 - 2027/28 Capital Improvement Program

This fund was established under section 2032(H)(2) of the Streets and Highway Code with the adoption of SB1. The Road Maintenance and Rehabilitation Act (RMRA) addresses maintenance on the local street and road system. Funding may be spent on road maintenance and rehabilitation, safety projects, railroad grade separations, complete street components, and traffic control devices.

Cash Balance Summary

	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Beginning Cash Balance	923,800	36,700	13,100	14,400	82,100	16,900
SB1 - Road Maintenance and Rehabilitation	3,174,200	3,237,700	3,302,500	3,368,600	3,436,000	3,504,700
Interest Earnings	40,000	40,000	100	400	100	100
Allocations	(1,300)	(1,300)	(1,300)	(1,300)	(1,300)	(1,300)
Capital Improvements	(4,100,000)	(3,300,000)	(3,300,000)	(3,300,000)	(3,500,000)	(3,500,000)
Total Resources Available for Future Projects	36,700	13,100	14,400	82,100	16,900	20,400

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
1	Pavement Rehabilitation - SB1: Rehabilitation of various streets throughout the City of Visalia based on priority list established by the Pavement Management System (PMS). Street rehabilitation is done to improve the ride quality, add structural strength, and extend the life expectancy of a street. This project is a longer lasting and important maintenance project for City streets that will be coordinated with the Public Works Department through analysis from Pavement Management System.	Chantha Chap	CP0290	- -	N/A	4,100,000	3,300,000	3,300,000	3,300,000	3,500,000	3,500,000
						4,100,000	3,300,000	3,300,000	3,300,000	3,500,000	3,500,000

Notes: Multi-funded means this is only this fund's portion of the total amount budgeted. These projects are funded from multiple sources and the full budget is shown in the "Multi-Fund 301".

The Budget Impact column shows the estimated annual impact of the project on the operating budget for maintenance once completed or purchased.

- - No Annual Maintenance Costs (or no increase over existing cost)

\$ Annual Maintenance cost is \$5,000 or less

\$ \$ Annual Maintenance costs is \$5,000 to \$25,000

\$ \$ \$ Annual Maintenance costs is over \$25,000. These projects will be explained in detail in the project description.

\$ \$ \$ \$ Project will result in savings as described in project description

Attachment A: Summary of Proposed Capital Projects by Fund

Measure R Local - 131 (2410) 2022/23 - 2027/28 Capital Improvement Program

This fund receives monies from the local portion of the Measure R ½ cent Transportation Sales Tax approved by voters of Tulare County in November of 2006. Local agencies collectively receive 1/3 of all Measure R revenues for local projects. These funds are to be expended in accordance with the Measure R expenditure plan as administered by the Tulare County Transportation Authority (TCTA).

Cash Balance Summary

	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Beginning Cash Balance	908,500	1,952,600	1,593,900	1,011,200	984,000	(1,218,800)
Measure R Local Funding	3,468,600	3,538,000	3,608,800	3,681,000	3,754,600	3,829,700
Investment Earnings	30,000	30,000	10,000	9,700	(12,100)	6,800
Allocations	(34,700)	(34,700)	(35,400)	(36,100)	(36,800)	(37,500)
Capital Improvements	(2,419,800)	(3,892,000)	(4,166,100)	(3,681,800)	(5,908,500)	(1,891,800)
Total Resources Available for Future Projects	1,952,600	1,593,900	1,011,200	984,000	(1,218,800)	688,400

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
1	Install Traffic Monitoring Cameras: Install ten (10) traffic monitoring cameras at intersections every 2 years for the purpose of monitoring traffic in real time. This project would add traffic monitoring cameras to the existing traffic management system. This will aide in the analysis of traffic to be used to modify the function of a single intersection or a group of intersections. It will also allow for real time verification of changes made through the Traffic Management System (TMS).	Eric Bons	CP0294	--	N/A	370,000					
2	Design - Major Street Rehab - Conyer St: Design for the rehabilitation of Conyer Street from Walnut Avenue to Main Street. Design will include rehabilitating the existing pavement, upgrading the traffic signals with video detection cameras, repairing damaged curb and gutter, constructing sidewalks, upgrading existing curb ramps to meet American with Disabilities Act (ADA) standards, and restriping to add buffered Class 2 bike lanes throughout. This project will allow staff to complete design and apply for grant funds to fund the cost of construction. Potential grant funding source includes the Surface Transportation Block Grant Program (Federal).	Chantha Chap	PWC100	--	D15	300,000					
3	Lovers Lane Center Median from Cherry to K Ave: Construction of landscape median surrounding the existing median trees along Lovers Lane between Cheery Avenue and K Avenue. This project has been identified to protect the landscaping in the center median to protect the motoring public from entering the landscape inadvertently.	Diego Corvera	PWCP19	--	D6	265,000					
4	Miscellaneous Sidewalk Improvements: Install sidewalk and pedestrian ramps along collector/arterial roadways within existing developed neighborhoods to provide connectivity to existing improvements. The sidewalks will be installed within the existing right of way where possible with the potential for minor right of way acquisitions. Project will provide connectivity where none exists. This will provide a safer mode for pedestrian travel where high vehicle volumes and speeds exist.	Frank Senteno	CP0403	--	N/A	240,000		240,000		240,000	

Attachment A: Summary of Proposed Capital Projects by Fund

Measure R Local - 131 (2410) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
5	Update Improvement Standards: Update the City's Engineering Improvement Standards and Specifications and Landscaping Improvement Standards and Specifications for optimization. Develop Visalia-specific green infrastructure, bicycle, and trail improvement standards and specifications for inclusion in the updated City Improvement Standards and City Improvement Specifications. Includes review and updated of existing City standards and specifications, and development of standard details for infiltration and treatment control Best Management Practices (BMPs). Includes update to Solid Waste standards in compliance with CalRecycle requirements. Multi-funded project total of \$312k from \$28k from General Fund prior year funds (001), \$228k Measure R Local (131) and \$62k Solid Waste (441).	Katherine Woodhull-Fuget	CP0472	--	N/A	228,000					
6	Vehicle Miles Traveled (VMT) Mitigation Nexus Study: Establish, identify and implement program-based VMT mitigation strategies for projects and development. This project will explore what mitigation measures may be available such as a Mitigation Bank, Exchange, and/or In-Lieu Fee program. In implementing SB743, the lead agency for environmental review under CEQA must now analyze and mitigate (where feasible) transportation impacts differently for both land development and transportation projects seeking a pathway to contribute to VMT mitigation where the cumulative contributions can pay for VMT reduction strategies that would not be feasible for individual projects to implement themselves.	Leslie Blair	PWCP16	--	N/A	220,000					
7	Local Roads Safety Plan (LRSP): Development of the Plan provides a proactive approach to identify, analyze and address roadway safety needs. Provides an avenue to prioritize improvements and respond to safety challenges. LRSP development would include outreach and formation of a stakeholder group; review of collision, traffic and roadway data; establishment of goals, priorities, and countermeasures; and implementation and assessment of the Plan. Plan will provide the framework for reducing roadway fatalities and serious injuries on public roadways. Plan required for the City to be eligible to apply for federal Highway Safety Improvement Program (HSIP) funds.	Leslie Blair	PWCP15	--	N/A	190,000					
8	Oak Avenue Extension: Extend Oak Ave from Tipton St to Burke St. This project includes installing street lights, pavement, curb, gutter, and sidewalk on both sides of the existing RR tracks on Oak Avenue. This will improve east downtown circulation from Administration and the Transit Center to the VECC and provides pedestrian access from west to east downtown and the VECC. Burke & Oak intersection completed in 2021. Additional funds will extend Oak Ave from Tipton and Burke.	Mike Porter	CP9924	\$	B15	181,600	1,284,500				

Attachment A: Summary of Proposed Capital Projects by Fund

Measure R Local - 131 (2410) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
9	ADA Self Evaluation and Transition Plan (City Right-of-Way): Update City's ADA transition plan and complete self-evaluation required per Title II, American's w/ Disabilities Act 1990. Analyze the City's right of way infrastructure. City facilities are not included in this project and will be addressed using other funding. The City's transition plan is outdated and needs to be updated to identify the areas of the City's infrastructure that need to be brought into compliance with current accessibility requirements. This transition plan will help fulfill the Federal self-evaluation and planning requirements for pedestrian facilities within the City public right of way.	Rebecca Keenan	CP8350	--	N/A	150,200					
10	Traffic Management Center (TMC) Software Support: To provide the software vendor maintenance, support, and licensing of the traffic management center systems to provide current updates. The maintenance and support of the traffic management system is a key component to the daily functionality of the TMC. The on-going support of this software allows for the communication between the TMC and the traffic signals which are connected to the traffic management network. Current maintenance and support package ends in FY2022-23. A three (3) year maintenance and support package will be purchased as a multi-year package provides for a lower per year cost.	Eric Bons	PWCP11	--	N/A	100,000			100,000		
11	Active Transportation Plan Update: Update the City's current ATP Plan to analyze and refine bikeway projects identified in the plan. As the city continues to grow and bicycle infrastructure is incorporated into various city projects, this project will update the Plan to better define the overall City non-vehicular network and to serve as a robust plan that can be referenced by Public Works staff and development to further expand the interconnected multi modal transportation routes throughout the City. Update will also review the bicycle classifications that the plan currently identifies to make non-vehicular commuting safer.	Diego Corvera	PWCP10	--	N/A	100,000					
12	Miscellaneous Pedestrian Improvements: Install crosswalks, curb ramps and other measures to provide safe modes of travel for pedestrians between points of interest and within existing developed neighborhoods and between points of interest. Pedestrian improvements will eliminate access barriers to mobility. This project would serve to develop preliminary engineering, design and construction as well as serve as the local matching funds for grants as they become available.	Leslie Blair	PWCP17	\$	N/A	25,000	120,000	138,000	138,000	138,000	138,000

Attachment A: Summary of Proposed Capital Projects by Fund

Measure R Local - 131 (2410) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
13	Traffic Signal Optimization Project (TSOP): This project is to optimize the traffic signal operations along various corridors within the City. The optimization of the traffic signals will minimize traffic delays and as a result reduce vehicle emissions. This is part of the traffic management system to aide the flow of traffic along the City corridors.	Eric Bons	CP0193	--	N/A	25,000	35,000	25,000	35,000	25,000	35,000
14	Stop Sign Installations: Annual stop sign installation contract for arterial/collector intersections. As traffic volumes increase in the City, various intersections meet the California Manual of Uniform Traffic Control Devices criteria for stop signs. When criteria is met, stop signs should be installed in a timely manner.	Wyndi Ferguson	CP0037	\$	N/A	25,000	25,000	25,000	25,000	25,000	25,000
15	Shirk St. & Walnut Ave Roadway Improvements: Complete roadway improvements along south side of Walnut Avenue for 700± feet east of Shirk Street and Shirk Street for 1,400± feet south of Walnut Ave in conjunction with Shirk St and Walnut Ave traffic signal project (CP0045). Restripe Walnut Ave from Teddy St to Shirk St. Widen north side of Walnut Ave for 350± feet east of Roeben Ave removing west bound travel lane restriction to improve traffic flow. Includes traffic signal interconnect conduit within Walnut between Akers and Shirk. Improvements associated with the Valley Oaks Subdivision. Project total of \$2.8 m from \$2.02m from TIF (241), including prior year funding and \$780k for Measure R Local (131).	Eric Bons	CP0196	\$	C12		780,000				
16	Design - Major Street Rehab - Giddings St: Design for the rehabilitation of Giddings Street from Whitendale Avenue to Murray Avenue. Design includes rehabilitating the existing pavement, upgrading the traffic signals with video detection cameras, adding signal interconnect conduits, repairing damaged curb and gutter, constructing sidewalks, upgrading existing curb ramps to meet American with Disabilities Act (ADA) standards, and restriping to add buffered Class 2 bike lanes throughout. This project will allow staff to complete design and apply for grant funds to fund the cost of construction. Potential grant funding source includes the Surface Transportation Block Grant Program (Federal).	Chantha Chap	PWCP101	--	D16		300,000				
17	Mooney & Ferguson Traffic Signal: Install traffic signal at Mooney and Ferguson. The current all-way stop intersection meets the traffic signalization criteria defined by the California Manual on Uniform Control Devices (MUTCD) as determined through the data collected with the Annual Traffic Data Collection Contract. Based on MUTCD traffic signal warrant analysis and the City of Visalia traffic signalization procedures, the intersection of Mooney and Ferguson meets the criteria for a traffic signal. Project includes design and construction. (Multi-funded project total of \$800k from \$550K LTF (281) and \$250 Measure R Local (131).)	Casey Chu	PWC103	\$	B22		250,000				

Attachment A: Summary of Proposed Capital Projects by Fund

Measure R Local - 131 (2410) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
18	Accessibility Upgrades & Barrier Removal - This project will provide funding to address identified accessibility issues in the public right-of-way to help ensure the City complies with Federal and State requirements. The CIP complies with the City's existing ADA Transition Plan and is a critical component to implement upcoming revisions to the transition plan. Costs shown are sufficient to improve 4 curb ramps at one intersection every other year. However funds from this CIP can be used to address smaller concerns, or can be combined with a larger multi-funded, street improvement project located in identified areas of concern.	Rebecca Keenan	CP0292	--	N/A		218,800		218,800		218,800
19	NW Quadrant At-Grade Railroad Crossing: Study to determine location for new at-grade crossing (possibly Linwood St, Chinowth St, or Preston/Roeben St) & abandonment of existing crossing. Installation of new pavement, curb, gutter, sidewalk, traffic signal, railroad warning devices, signage and striping at recommended location. Upgrade railroad tracks. Includes public outreach, abandonment of existing RR crossing and moderate updates to two adjacent intersections north & south of new crossing. Need for a new crossing with the increased development in NW quadrant and the limited at-grade railroad crossings. FY23/24 Preliminary Engineering & Environmental, PS&E, Right of Way and Permitting. FY26/27 Construction. Multi funded project total of \$4.8m from \$2.4m Measure R Local (131) and \$2.4m Transportation Impact Fees (241).	Rebecca Keenan	PWCP79	--	A8		200,000	286,900		1,938,500	
20	Battery Backup System Installation Traffic Signals: Install battery backup systems to the existing traffic signals at various locations throughout the City. The battery backup system will allow each traffic signal to operate during power outages to provide for driver safety. Older traffic signals need to be retrofitted with the system. Install battery backup systems on 12 traffic signals per year, installation of battery backup systems will be complete within 4 years.	Casey Chu	CP0226	\$	N/A		150,000		150,000		150,000
21	Traffic Signal Modifications at Lovers Lane and Walnut Ave: To accommodate four lanes of travel east and west along Walnut Ave and mitigate traffic impacts generated from the residential subdivision at the south east corner. Subdivision completed the full widening of Walnut Ave between Lovers Lane and McAulif St. Requires installation of additional signal poles and heads to accommodate additional lanes and Video detection typical for all new traffic signals. Also includes right-of-way acquisition which is being funded by CP9130. The City is responsible for traffic signal modifications. Multi-funded project total of \$755k from \$605k Transportation Impact (241) and \$150K Measure R Local (131).	Diego Corvera	PWCP78	\$	D7		150,000				

Attachment A: Summary of Proposed Capital Projects by Fund

Measure R Local - 131 (2410) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
22	Caldwell Signal Interconnect Fiber Optic (SICFO) Santa Fe to Lovers Lane: Install fiber optic cable connecting existing Intelligent Transportation System's (ITS) fiber optic cable at Caldwell Ave and Santa Fe St to Caldwell Ave and Lovers Lane. Includes installation of communication equipment and related supporting equipment at each signalized intersection along Caldwell Ave from Santa Fe St to Lovers Lane. Project will install the fiber optic cable within the conduit, installed as part of the Caldwell Ave Widening from Santa Fe to Lovers Lane Project (CP8268).	Eric Bons	PWCP13	--	D8		125,000	350,000			
23	Shirk and Ferguson Traffic Signal: Install traffic signal at Shirk and Ferguson. The current all-way stop intersection meets the traffic signalization criteria defined by the California Manual on Uniform Control Devices (MUTCD) as determined through the data collected with the Annual Traffic Data Collection Contract. Based on MUTCD traffic signal warrant analysis and the City of Visalia traffic signalization procedures, the intersection of Shirk and Ferguson meets the criteria for a traffic signal. Project includes design and construction. (Multi-funded project total of \$800k from \$275K LTF (281) and \$525K Measure R Local (131).)	Casey Chu	PWC102	\$	A15		100,000	425,000			
24	Design Tower St (Rd 148) from Walnut Ave to Visalia Parkway (1.6 miles): Analyze options for a 4-lane, north-south arterial along Road 148 alignment. FY23/24 includes rough topo survey, geometric alignments, permitting requirements, right of way needs, estimates, project phasing. FY 25/26-27/28, includes Walnut to RR at K-Road Alignment (Rector Station) - CEQA, Design, RW, Permitting. Future Construction cost estimated to be \$5.9M. K Road to Caldwell & Caldwell to Visalia Parkway are located in Tier 2 & 3 of the Gen Plan and will be deferred to future years. Multi-funded project total of \$1.1m from \$567k Measure R Local (131) and \$567k Transportation Impact (241).	Rebecca Keenan	PWCP80	--	D9		92,100	475,400			
25	Design and Construct Tower St (Rd 148) from Cypress Ave to Walnut Ave (4200 feet): Analyze options for a 4-lane, north-south arterial along Rd 148 alignment, coordinating with future SR 198 interchange, SCE towers, Greenway Trail, Creeks, and other structures. FY23/24 includes rough topo survey, geometric alignments, permitting requirements, Caltrans coordination, right of way needs, estimates. FY 25/26-26/27 includes Tulare to Walnut - CEQA, Design, RW, Permitting; FY 27/28 Construction Ph 1 [Tulare to Walnut, (2500 feet)] approx. \$4.5M. Due to continuing development in east Visalia, around State Route 198, another north/south arterial is needed. Multi funded project total of \$5.4m from \$2.7m Measure R Local (131) and \$2.7m Transportation Impact (241).	Rebecca Keenan	PWCP81	\$	D10		61,600	437,800		2,217,000	

Attachment A: Summary of Proposed Capital Projects by Fund

Measure R Local - 131 (2410) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
26	Walnut Ave Communications Connectivity: Installation of conduit and fiber optic cable as an extension of the Intelligent Transportation System (ITS) to provide a City owned fiber optic connection to the Water reclamation Facility (WRF). Provides connection between existing fiber optic communication infrastructure at the intersection of Akers St and Walnut Ave and the intersection of Walnut Ave and Plaza Dr; and bridges the communication gap in Walnut Avenue from Akers Street to Plaza Drive. Multi-funded project total of \$1.38m from \$530k WWTP (431), \$350k Info Services (511), and \$500k Measure R Local (131).	Eric Bons	PWCP61	--	C4			500,000			
27	Main-Center One-Way Conversion: Main Street & Center Avenue one-way conversion between Santa Fe Street and Ben Maddox Way forming existing two-way streets to a one-way couplet. Project includes design engineering and construction based upon the Traffic Study for Main/Center One-Way Conversion project completed in 2020 (CP0296). Multi funded project total of \$2.15m from \$410k Measure R Local (131) and \$1.74m Gas Tax (111)	Leslie Blair	PWCP77	--	B13			410,000			
28	Preliminary Design for Grant Opportunities: Preliminary engineering and design work necessary for potential future capital projects. This project is important to fund pre-design work on future projects involving State and/or Federal grant funds. This project will allow staff to complete design and apply for grant funds to fund the cost of construction for projects including road rehabilitation of major collector and arterial roadways, improvements on pedestrian and bicycle infrastructure and other improvements that address local transportation needs. Potential grant funding source includes the Surface Transportation Block Grant Program (Federal).	Frank Senteno	PWCP18	--	N/A			300,000	300,000	300,000	300,000
29	Laura Avenue Widening at Garden Street: Install new sidewalk, curb and gutter, curb ramp, and asphalt along Laura Avenue near the intersection of Laura Avenue and Garden Street. Project will widen Laura Avenue to accommodate two way travel, work will also include new sidewalk, curb and gutter, curb ramp.	Cody McLaughlin	PWCP99	--	D14			273,000			
30	Akers Street Traffic Signal Interconnect: Install conduit and fiber optic cable in Akers between Walnut and Goshen interconnecting the traffic signals with the traffic management system. The traffic signal interconnect will create a fiber optic cable back bone structure that will extend the existing interconnection system to be incorporated into the traffic management system. This will provide real time traffic information to modify traffic flow patterns and work in conjunction with the Caltrans controlled signalized intersection located at Akers St. & Mineral King Ave. and at Akers St. & Noble Ave.	Eric Bons	CP0194	--	A9			150,000	1,450,000		

Attachment A: Summary of Proposed Capital Projects by Fund

Measure R Local - 131 (2410) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
31	Traffic Signal Modification at Giddings & Mineral King: Replacement of the existing traffic signal with a fully actuated traffic signal, incorporated intelligent transportation equipment, modification of the existing pedestrian ramps to meet the current Americans with Disabilities Act requirements and construction of the associated roadway improvements. The existing traffic signal, originally constructed in the mid 1960's, is need of an update to meet the current traffic signal requirements per the California Manual on Uniform Traffic Control Devices.	Eric Bons	PWCP12	--	B16			130,000	640,000		
32	Traffic Control Signal Installation: Construct one traffic signal per year as identified by the data collected with the Annual Traffic Data Collection contract. The data will be analyzed to determine which intersections meet the traffic signal criteria as defined by the California Manual on Uniform Traffic Control Devices. Future funds will address one intersection per year. Based on MUTCD warrant analysis and City of Visalia ranking systems. Multi-funded annual project from LTF (281) and Measure R Local (131).	Casey Chu	CP0038	--	N/A				525,000	525,000	525,000
33	Signal Interconnect and Sanitary Lift Station Connectivity: Provide a City owned fiber optic connection to existing sanitary lift stations near airport terminal building and sanitary lift station at north side of golf course along south side of Hwy198 and extend signal interconnect to Crowley Ave at Plaza Drive. Involves utilization of existing communication conduit and installation of new conduit from Airport Administration Building to each of two sanitary lift stations. Equips each sanitary lift station for remote monitoring of sanitary flows. Contingent upon receiving state/federal grant funds. Multi-funded total of \$1.6m from \$430k WWTP (431) and \$1.2m Fed/State Grant (281). <u>Project will not move forward without grant funding.</u>	Eric Bons	PWCP62	--	C7				100,000		
34	Goshen Widening Ben Maddox to Burke: Widen to full 84 foot 2-lane collector. Includes paving, curb, gutter, sidewalk, ADA ramps, street lights, bike lanes, landscaping and irrigation conduit. Requires coordination with utilities to relocate poles, property owners to relocate and install drive approaches, and acquire ROW. Improves traffic circulation and pedestrian accessibility. Will positively impact development for Civic Center and commercial parcels within the area. Multi funded project total of \$2.36m from \$1.0m Measure R Local (131) and \$1.36m Transportation Impact (241).	Mike Porter	PWCP83	\$	B17					500,000	500,000
						2,419,800	3,892,000	4,166,100	3,681,800	5,908,500	1,891,800

Notes: Multi-funded means this is only this fund's portion of the total amount budgeted. These projects are funded from multiple sources and the full budget is shown in the "Multi-Fund 301".

The Budget Impact column shows the estimated annual impact of the project on the operating budget for maintenance once completed or purchased.

-- No Annual Maintenance Costs (or no increase over existing cost)

\$ Annual Maintenance cost is \$5,000 or less

\$\$ Annual Maintenance costs is \$5,000 to \$25,000

\$\$\$ Annual Maintenance costs is over \$25,000. These projects will be explained in detail in the project description.

\$\$\$\$ Project will result in savings as described in project description

Attachment A: Summary of Proposed Capital Projects by Fund

Measure R Regional - 133 (2430) 2022/23 - 2027/28 Capital Improvement Program

This fund receives monies from the Regional Portion of the Measure R 1/2 cent Transportation Sales Tax approved by voters of Tulare County in November of 2006. Regional projects in Tulare County collectively receive 1/2 of all Measure R revenues. The Tulare County Transportation Authority (TCTA) administers the Measure R expenditure plan which restricts how this money can be spent.

Cash Balance Summary

	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Beginning Cash Balance	0	0	0	0	0	0
Measure R Regional Funding			1,600,000	420,000	3,760,000	
Capital Improvements	0	0	(1,600,000)	(420,000)	(3,760,000)	0
Total Resources Available for Future Projects	0	0	0	0	0	0

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
1	Riggin Widening Shirk to Akers: Includes curb and gutter, pavement, medians, street lights, and other infrastructure such as fire hydrants, drainage inlets and utility relocation. This segment, along with other CIP's to widen Riggin will help to complete the 4-lane arterial from Hwy 63 to Hwy 99 on Riggin Ave, providing a major East-West corridor along the North side of Visalia. Multi-funded project total of \$10.5m from \$1.6m Measure R Regional (133) and \$8.9m Transportation Impact (241).	Diego Corvera	PWCP85	\$	A10			1,600,000			
2	Shirk/SR-198 Interchange (Long-Term Improvements): Design and construction of the ultimate design for the interchange at SR198 and Shirk Street. Caltrans, the lead agency, will control the process of project development and delivery beginning in FY2025-26. FY 25/26 will begin development of a Project Initiation Document (PID), followed by a Project Approval & Environmental Document (PA&ED) to start in FY 26/27. Design will follow completion of the PA&ED and construction projected to start FY2035-36. The PID analysis will incorporate new traffic data following completion of the Shirk Capacity Enhancement project (CP0369), any near-term operational projects, and explore long-term interchange design options.	Rebecca Keenan	PWCP21	--	C11				420,000	3,760,000	
						0	0	1,600,000	420,000	3,760,000	0

Notes: Multi-funded means this is only this fund's portion of the total amount budgeted. These projects are funded from multiple sources and the full budget is shown in the "Multi-Fund 301".

The Budget Impact column shows the estimated annual impact of the project on the operating budget for maintenance once completed or purchased.

-- No Annual Maintenance Costs (or no increase over existing cost)

\$ Annual Maintenance cost is \$5,000 or less

\$\$ Annual Maintenance costs is \$5,000 to \$25,000

\$\$\$ Annual Maintenance costs is over \$25,000. These projects will be explained in detail in the project description.

\$\$\$\$ Project will result in savings as described in project description

Attachment A: Summary of Proposed Capital Projects by Fund

Transportation Impact Fees - 241 (2520) 2022/23 - 2027/28 Capital Improvement Program

This fund is derived from fees collected at the time of building permit issuance. Funds can be used only for new street improvements and expansion of transportation facilities related to growth.

Cash Balance Summary

	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Beginning Cash Balance	6,916,800	6,265,400	7,453,000	8,685,800	(2,363,000)	(6,854,700)
Transportation Impact Fees	6,263,100	6,388,400	6,516,200	6,646,500	6,779,400	6,915,000
Interest Earnings (Expense)	100,000	73,800	86,000	(23,400)	(67,900)	(156,500)
Operating Expenditures and Allocations	(35,300)	(35,300)	(36,000)	(36,700)	(37,400)	(38,100)
Capital Improvements	(6,979,200)	(5,239,300)	(5,333,400)	(17,635,200)	(11,165,800)	(15,673,600)
Total Resources Available for Future Projects	6,265,400	7,453,000	8,685,800	(2,363,000)	(6,854,700)	(15,807,900)

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
1	Riggin Widening - Kelsey to Shirk: Widen Riggin Ave between Kelsey Street and Shirk Road from an undivided two-lane road to a four-lane divided road with median. Install drainage swale, street lights and interconnect conduit. Project will move forward once EDA grant is received and bonding from TCAG is successful. Widening of Riggin Ave will improve mobility of traffic for the industrial park and the surrounding businesses while reducing congestion. Multi funded project total of \$10.975m from \$2.2m Meas R Regional prior year (133), \$4.77m Transportation Impact (\$2.4m prior year) (241), and \$4m EDA Grant, <i>contingent upon award</i> (281).	Diego Corvera	CP0398	\$	A11	2,375,000					
2	Santa Fe Environmental Cleanup: Project includes removing the elevated railroad embankment along the future Santa Fe Street Corridor from Houston to Riggin. The corridor will be excavated down to a preliminary grading limit so that there will be minimal earthwork for the construction of Santa Fe Street. The elevated railroad embankment runs parallel to the trail currently under construction along the eastern most portion of the future street and rises over the trail 4-5' in some locations. This presents a safety concern with limited visibility for trail users. This project will simplify the future street project. Project total of \$2.6m includes prior year funding of \$750K.	Katherine Woodhull-Fuget	CP0308	\$	B18	1,886,200					
3	Developer Reimbursements: This CIP is for reimbursement of public assets constructed in the public right of way. The continued and ongoing maintenance of the improvements completed under this CIP is maintained by a separate City CIP.	Paul Bernal	CP9130	--	N/A	1,100,000	1,100,000	1,100,000	1,100,000	1,100,000	1,100,000

Attachment A: Summary of Proposed Capital Projects by Fund

Transportation Impact Fees - 241 (2520) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
4	Visalia Parkway Improvements-North Half: Extend Visalia Parkway between Stonebrook Street and Court Street. Visalia Parkway is identified as an arterial transportation corridor in the Circulation Element of the City's General Plan. Construct the northerly half to provide one lane of travel in each direction within current right of way. Improves mobility and connectivity of the southern shopping district and neighborhoods in the south east community of the City. Future CIP, Visalia Parkway & Santa Fe Intersection, will connect Visalia Parkway from Court to Santa Fe, and establish future alignment east towards Ben Maddox. Multi funded project total of \$3.55m from \$2.08m Transportation Impact (241) and \$1.459m State Transportation (281-LTF).	Casey Chu	PWCP82	\$	D11	631,400		1,458,200			
5	Riggin Widening from Mooney to Conyer: The project would include curb & gutter, pavement, medians, street lights, and other infrastructure such as fire hydrants and drainage inlets. This project will also construct a frontage road along this stretch to protect the Riggin facing residents. Utility relocation would also be needed. Construction of this segment will help to complete the 4 lane arterial from Hwy 63 to Hwy 99 on Riggin Ave; providing a major East-West corridor along the North side of Visalia.	Diego Corvera	CP0473	\$	B20	454,000					
6	Traffic Signal Modifications at Lovers Lane and Walnut Ave: To accommodate four lanes of travel east and west along Walnut Ave and mitigate traffic impacts generated from the residential subdivision at the south east corner. Subdivision completed the full widening of Walnut Ave between Lovers Lane and McAulif St. Requires installation of additional signal poles and heads to accommodate additional lanes and Video detection typical for all new traffic signals. Also includes right-of-way acquisition which is being funded by CP9130. The City is responsible for traffic signal modifications. Multi-funded project total of \$755k from \$605k Transportation Impact (241) and \$150K Measure R Local (131).	Diego Corvera	PWCP78	\$	D7	160,000	445,000				
7	Houston-Santa Fe Roundabout Landscaping - Design and install irrigation and landscaping within the Houston-Santa Fe roundabout. Design will include maintenance access. To visually improve the aesthetics of the surrounding community to promote economic growth.	Frank Senteno	CP0307	\$	B21	157,600					

Attachment A: Summary of Proposed Capital Projects by Fund

Transportation Impact Fees - 241 (2520) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
8	Linwood Street Embankment: Design and construction for the embankment of the future extension of Linwood Street north of Riggan Avenue. The embankment will be approximately located within and along the westerly portion of the current Modoc Basin footprint. Establishment and rough grading of the Linwood Street alignment north of Riggan Avenue is needed in order to accommodate access to the Tier II Urban Development Boundary north of the Modoc Basin. This project will build up the embankments and rough grade in preparation for the future extension of Linwood Street with preliminary engineering and design in FY 22/23 and construction in FY 23/24.	Cody McLaughlin	PWCP93	--	A7	113,000	254,500				
9	TIF Administration: This CIP funds staff time used to update the circulation element and to implement the transportation impact fee program. The supplies used by staff are budgeted under a separate City CIP. This CIP also funds of public assets constructed in the public right of way. The continued and ongoing maintenance of the improvements completed under this CIP is maintained by a separate City CIP.	Paul Bernal	CP9633	--	N/A	100,000	10,000	10,000	10,000	10,000	100,000
10	East Side Regional Park EIR: Design and environmental work consisting of park master planning, preliminary engineering for recharge facilities and project level EIR. The completed design and environmental work will put the project in a good position to seek grant funding opportunities with a well planned package. The additional appropriation is needed to add a noise study and to expand the traffic impact study to meet Caltrans' requirements for impacts to SR-198 and SR-216. Multi funded Project total of \$340k is funded from \$265k Park Impact Fee (211), \$62k Groundwater Recharge (224), \$6k Transportation Impact (241) and \$7k Waterways (261), all of which includes prior year funding.	Rebecca Keenan	CP0123	--	N/A	2,000					
11	Design Tower St (Rd 148) from Walnut Ave to Visalia Parkway (1.6 miles): Analyze options for a 4-lane, north-south arterial along Road 148 alignment. FY23/24 includes rough topo survey, geometric alignments, permitting requirements, right of way needs, estimates, project phasing. FY 25/26-27/28, includes Walnut to RR at K-Road Alignment (Rector Station) - CEQA, Design, RW, Permitting. Future Construction cost estimated to be \$5.9M. K Road to Caldwell & Caldwell to Visalia Parkway are located in Tier 2 & 3 of the Gen Plan and will be deferred to future years. Multi-funded project total of \$1.1m from \$567k Measure R Local (131) and \$567k Transportation Impact (241).	Rebecca Keenan	PWCP80	--	D9		92,000	475,300			

Attachment A: Summary of Proposed Capital Projects by Fund

Transportation Impact Fees - 241 (2520) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
12	Design and Construct Tower St (Rd 148) from Cypress Ave to Walnut Ave (4200 feet): Analyze options for a 4-lane, north-south arterial along Rd 148 alignment, coordinating with future SR 198 interchange, SCE towers, Greenway Trail, Creeks, and other structures. FY23/24 includes rough topo survey, geometric alignments, permitting requirements, Caltrans coordination, right of way needs, estimates. FY 25/26-26/27 includes Tulare to Walnut - CEQA, Design, RW, Permitting; FY 27/28 Construction Ph 1 [Tulare to Walnut, (2500 feet)] approx. \$4.5M. Due to continuing development in east Visalia, around State Route 198, another north/south arterial is needed. Multi funded project total of \$5.4m from \$2.7m Measure R Local (131) and \$2.7m Transportation Impact (241).	Rebecca Keenan	PWCP81	\$	D10		79,300	420,400		2,217,000	
13	Shirk St. & Walnut Ave Roadway Improvements: Complete roadway improvements along south side of Walnut Avenue for 700± feet east of Shirk Street and Shirk Street for 1,400± feet south of Walnut Ave in conjunction with Shirk St and Walnut Ave traffic signal project (CP0045). Restripe Walnut Ave from Teddy St to Shirk St. Widen north side of Walnut Ave for 350± feet east of Roeben Ave removing west bound travel lane restriction to improve traffic flow. Includes traffic signal interconnect conduit within Walnut between Akers and Shirk. Improvements associated with the Valley Oaks Subdivision. Project total of \$2.8 m from \$2.02m from TIF (241), including prior year funding and \$780k for Measure R Local (131).	Eric Bons	CP0196	\$	C12		1,820,000				
14	Visalia Parkway & Santa Fe Intersection (Design): Includes design, environmental, and right of way acquisition for the proposed roadway intersection. Design will include roadway and intersection improvements, railroad crossing, culvert construction, and incorporate the existing Santa Fe Bike Trail and the roadway segment of Visalia Parkway between Court and the intersection. Project design and environmental studies in FY 23/24 with Right-of-Way acquisition following in FY 24/25. Construction in separate CIP deferred pending available funding. Project will further transportation connectivity through the ongoing development of the arterial ring road concept that includes Visalia Parkway, Tower, Riggins, and Shirk by connecting Visalia Parkway to Santa Fe Street.	Casey Chu	PWCP25	\$	D12		855,000	297,500			

Attachment A: Summary of Proposed Capital Projects by Fund

Transportation Impact Fees - 241 (2520) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
15	NW Quadrant At-Grade Railroad Crossing: Study to determine location for new at-grade crossing (possibly Linwood St, Chinowth St, or Preston/Roeben St) & abandonment of existing crossing. Installation of new pavement, curb, gutter, sidewalk, traffic signal, railroad warning devices, signage and striping at recommended location. Upgrade railroad tracks. Includes public outreach, abandonment of existing RR crossing and moderate updates to two adjacent intersections north & south of new crossing. Need for a new crossing with the increased development in NW quadrant and the limited at-grade railroad crossings. FY23/24 Preliminary Engineering & Environmental. FY24/25 & FY25/26 PS&E, Right of Way and Permitting. FY26/27 & FY27/28 Construction. Multi funded project total of \$4.8m from \$2.4m Measure R Local (131) and \$2.4m Transportation Impact Fees (241).	Rebecca Keenan	PWCP79	--	A8		486,800			1,938,300	
16	Tower Street-Mineral King to St. John's Parkway (Prelim Design): Preliminary design for construction of approximately 6,400 ft of Tower Street from Mineral King to St. John's Parkway, excluding the new Interchange to SR 198 for access to park/basins. Will be a 4-lane, north-south arterial along the Rd 148 alignment, in coordination with future interchange at SR 198 and SR216 (Houston Ave); the SCE towers; and the Greenway Trail. Includes construction of new culvert crossing Mill Creek (Army Corp Permit). Development in east Visalia, around SR 198, requires another north/south arterial. No construction funds in this project. Multi funded project total of \$194k from \$96k Transportation Impact (241) and \$97k Parks & Rec (211).	Rebecca Keenan	PWCP84	--	B8		96,700				
17	Santa Fe Street - Houston to Riggan (DESIGN): Project will develop plans, specs and estimate for construction of a 4-lane, north-south, arterial along the old Santa Fe Railroad alignment where City owns ROW and tracks have been removed. Project also includes environmental review (CEQA and NEPA), public outreach and construction easement acquisition. By preparing design plans for construction of the future street segment, the street project will be "shovel-ready" and will allow the City to pursue more grant funding opportunities for construction of the street. CP0308 Santa Fe Environmental Cleanup needs to be completed before staff can proceed with design of the road extension.	Katherine Woodhull-Fuget	CP0040	--	B18			1,203,000	485,000		

Attachment A: Summary of Proposed Capital Projects by Fund

Transportation Impact Fees - 241 (2520) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
18	Northside Scenic Corridor - Hillsdale Park/Storm Basin: Develop Northside Highway 198 Open Space Setback Area (east of Shirk) to serve as passive open space and naturally shaped drainage basin for existing and planned development north of Highway 198 and both east and west of Shirk Street. Basin could receive water from Mill Creek for storm layoff and recharge purposes. Development of this is the first section of setback will include planning for other setback areas to the south and the west. (Multi Funded: Total of \$6.33m, including prior year funding, from \$2.65m Storm Sewer (221), \$1.77m Parks & Recreation (211), \$1m Transportation Impact (241), and \$900k Waterways Capital (261).	Katherine Woodhull - Fuget	CP9719	\$\$	A3			319,000	690,000		
19	American Street Extension Goshen to Riggan: Includes curb & gutter, sidewalks, curb ramps and extension of street. Project requires award of an EDA grant or cost sharing with the County to move forward. FY 24/25 (Staff submit grant application), FY 25/26 (Design and ROW), FY 26/27 - FY 27/28 (Construction). Improves mobility of traffic for the industrial park and the surrounding businesses while reducing congestion and increase the level of service for the roadway. Multi funded project total of \$7.4m from \$1.2m Transportation Impact (241) and \$6.2m EDA grant (281). Project will not move forward without grant funding.	Chantha Chap	CP0309	\$\$	A12			50,000	1,140,000		
20	Riggan Widening Shirk to Akers: Includes curb and gutter, pavement, medians, street lights, and other infrastructure such as fire hydrants, drainage inlets and utility relocation. This segment, along with other CIP's to widen Riggan will help to complete the 4-lane arterial from Hwy 63 to Hwy 99 on Riggan Ave, providing a major East-West corridor along the North side of Visalia. Multi-funded project total of \$10.5m from \$1.6m Measure R Regional (133) and \$8.9m Transportation Impact (241).	Diego Corvera	PWCP85	\$	A10				8,945,000		
21	Visalia Parkway & Santa Fe Intersection (Construction): This project will construct a new roadway intersection at Visalia Parkway & Santa Fe Street. The work will entail roadway and intersection improvements, railroad crossing, culvert construction, and incorporate the existing Santa Fe Bike Trail into the intersection. Construction will also include the roadway segment of Visalia Parkway between Court Street and the intersection. This project will further transportation connectivity through the ongoing development of the arterial ring road concept that includes Visalia Parkway, Tower, Riggan, and Shirk by connecting Visalia Parkway to Santa Fe Street.	Casey Chu	PWCP25	\$	D12				2,682,500	5,652,100	

Attachment A: Summary of Proposed Capital Projects by Fund

Transportation Impact Fees - 241 (2520) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
22	Tulare Avenue Extension to Tower Road: Construct Tulare Avenue from Arroyo Street to Road 148 (Tower Road) and extend Tower Road from Tulare Avenue to just north of Harvard. The section of Tulare Avenue from Lovers Lane to McAuliff was completed in 2018. Improve traffic circulation. This project was intended to be constructed with the "NEW" project to extend Tulare Ave from Lovers Lane to McAuliff and will provide additional access and help alleviate congestion on Walnut Avenue and McAuliff. Additional subdivision development in this area will benefit greatly from this additional N/S access to Walnut Avenue.	Mike Porter	PWCP60	\$	D13				1,232,700		
23	Ben Maddox Widening: Widen Ben Maddox from Center Avenue to Goshen Avenue. Project includes widening Ben Maddox to full 110 foot, 4 lane divided arterial. Improvements include paving, curb and gutter, sidewalk, ADA ramps, street lights, landscaping and irrigation conduit. Project requires coordination with utilities to relocate poles, property owners to relocate and install drive approaches, and acquire ROW. Note: UUD \$1.3M not included. Improves traffic circulation and pedestrian accessibility. Widening will also have a positive impact on development for the Civic Center and commercial parcels within the area.	Mike Porter	PWCP58	\$	B19				850,000	248,400	2,009,900
24	Goshen Widening Ben Maddox to Burke: Widen to full 84 foot 2-lane collector. Includes paving, curb, gutter, sidewalk, ADA ramps, street lights, bike lanes, landscaping and irrigation conduit. Requires coordination with utilities to relocate poles, property owners to relocate and install drive approaches, and acquire ROW. Improves traffic circulation and pedestrian accessibility. Will positively impact development for Civic Center and commercial parcels within the area. Multi funded project total of \$2.36m from \$1.0m Measure R Local (131) and \$1.36m Transportation Impact (241).	Mike Porter	PWCP83	\$	B17				500,000		850,000
25	Construct Santa Fe Street Houston to Riggins: Construct 4-lane, north-south, arterial along the old Santa Fe Railroad where City owns right of way, and tracks have been removed. Includes, grading, new structural section, curb and gutter, storm drain system, median, street lighting, striping, landscaping, and block wall installation. Also included is single box culvert (100' long) over Modoc Ditch, upgrading signal at Riggins intersection, signalized pedestrian crosswalk at Modoc Ditch Trail crossing, and interconnect conduit for the entire length of project. Staff to pursue grant funding opportunities and project will not move forward without an award of grant funds.	Katherine Woodhull-Fuget	PWCP59	\$\$	B18						11,033,700

Attachment A: Summary of Proposed Capital Projects by Fund

Transportation Impact Fees - 241 (2520) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
26	Southside Scenic Corridor: Develop basin on south side of SR-198, east of Shirk St as linear, landscaped basin with functional passive open space to receive City storm water flows from Mill Creek through either Persian-Watson Ditch or a pipeline to the north under SR-198. Phase 1, implement with Sierra Village development expansion to relocate existing City basin. Requires acquisition of 20 acres to the east of Phase 1 improvements. Included in the west side storm water lay-off plan. Multi funded project total of \$4.3m from \$1.4m Parks & Rec (211), \$2.2m Storm Sewer Construction (221), and \$580k Transportation Impact (241).	Katherine Woodhull - Fuget	PWCP75	\$	C8						580,000
						6,979,200	5,239,300	5,333,400	17,635,200	11,165,800	15,673,600

Notes: Multi-funded means this is only this fund's portion of the total amount budgeted. These projects are funded from multiple sources and the full budget is shown in the "Multi-Fund 301".

The Budget Impact column shows the estimated annual impact of the project on the operating budget for maintenance once completed or purchased.

-- No Annual Maintenance Costs (or no increase over existing cost)

\$\$\$

Annual Maintenance costs is over \$25,000. These projects will be explained in detail in the project description.

\$ Annual Maintenance cost is \$5,000 or less

\$\$\$\$

Project will result in savings as described in project description

\$\$ Annual Maintenance costs is \$5,000 to \$25,000

Attachment A: Summary of Proposed Capital Projects by Fund

Landscape and Lighting - 273 (2730) 2022/23 - 2027/28 Capital Improvement Program

Funding from property tax assessments on property owned within each Landscape and Lighting (L&L) District for maintenance of common areas, street maintenance and landscaping. Funds to be used only within each individual L&L District for costs included in the engineer's report prepared at the time of the creation of each L&L. Increases in the assessment amount paid by each property owner or changes to the approved expenditures must be voted upon by the owner's of the property located within each L&L.

Cash Balance Summary

	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Beginning Cash Balance	5,412,400	5,922,000	5,932,800	6,188,100	6,034,200	6,277,500
Special Service District Maintenance Assessments (Streets Portion Only)	761,600	776,800	792,300	808,100	824,300	840,800
Capital Improvements	(252,000)	(766,000)	(537,000)	(962,000)	(581,000)	(1,057,000)
Total Resources Available for Future Projects	5,922,000	5,932,800	6,188,100	6,034,200	6,277,500	6,061,300

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
1	Reclamite Landscape & Lighting Districts: Placement of petroleum based emulsion to rejuvenate drying and worn pavement surfaces. Cost effective pavement maintenance when performed routinely (within 5-7 years) after initial proper street installation and results in cost savings over the pavement life span compare to other methods of pavement maintenance or replacement. Allows rejuvenation of asphalt oils to drying and worn pavement surfaces. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions.	Casey Chu	CP8358	--	N/A	252,000	568,000	27,000	258,000	252,000	568,000
2	Crack Seal in Landscape and Lighting Districts: Placement of rubber type based product in cracked pavement to reduce water penetration, helping to maintain the structural strength of the asphalt and limit degradation, reduce potholes and prevent cracks from getting larger. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leave the City liable.	Casey Chu	CP0089	--	N/A		198,000	175,000	394,000	19,000	179,000
3	Overlay in Landscape and Lighting Districts: Placement of asphalt overlay on landscape and lighting streets as a cost effective way to extend the useful life of a street with asphalt in minor stages of deterioration. Pavement preservation and rehabilitation is essential in maintaining the largest City assets, public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions.	Chantha Chap	CP0090	--	N/A			310,000	310,000	310,000	310,000

Attachment A: Summary of Proposed Capital Projects by Fund

Landscape and Lighting - 273 (2730) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
4	Cape Seal in Landscape and Lighting Districts: Placement of an asphaltic emulsified seal coat and a chip seal on weathered and raveled pavement without substantial distress to extend the life of the pavement. Pavement preservation and rehabilitation are essential in maintaining the City's largest assets: public roadways. Without these funds, the roadways will deteriorate causing poor riding conditions and ultimately leave the City liable.	Casey Chu	PWCP27	- -	N/A			25,000			
						252,000	766,000	537,000	962,000	581,000	1,057,000

Notes: Multi-funded means this is only this fund's portion of the total amount budgeted. These projects are funded from multiple sources and the full budget is shown in the "Multi-Fund 301".

The Budget Impact column shows the estimated annual impact of the project on the operating budget for maintenance once completed or purchased.

- - No Annual Maintenance Costs (or no increase over existing cost)

\$ Annual Maintenance cost is \$5,000 or less

\$ \$ Annual Maintenance costs is \$5,000 to \$25,000

\$\$\$ Annual Maintenance costs is over \$25,000. These projects will be explained in detail in the project description.

\$\$\$\$ Project will result in savings as described in project description

Attachment A: Summary of Proposed Capital Projects by Fund

State Transportation - 281 (2810) 2022/23 - 2027/28 Capital Improvement Program

This fund is derived from 1/4 cent of statewide sales tax collected and returned to each County in compliance with the Local Transportation Development Act. First priority of funds is public transit (buses); remaining monies, as well as various discretionary revenues, may be used for road and street purposes, including bike/pedestrian facilities. This fund also receives monies from Tulare County Association of Government (TCAG), State Transportation Improvement Program (STIP), and Congestion Mitigation and Air Quality (CMAQ). These monies are to be used only for approved transportation projects.

Cash Balance Summary

	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
Beginning Cash Balance	2,145,000	2,145,000	2,145,000	686,000	686,000	686,000
LTD (Local Transportation Development Funds)	275,000	275,000	275,000	275,000	275,000	275,000
Other Grant Funding:						
EDA (Economic Development Administration)					3,107,200	3,107,200
State ATP (Active Transportation Program)		1,600,000				
Other Grant Funding (Various)			150,000	2,040,000	1,400,000	
Capital Improvements	(275,000)	(1,875,000)	(1,884,000)	(2,315,000)	(4,782,200)	(3,382,200)
Total Resources Available for Future Projects	2,145,000	2,145,000	686,000	686,000	686,000	686,000

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
1	Mooney & Ferguson Traffic Signal: Install traffic signal at Mooney and Ferguson. The current all-ways stop intersection meets the traffic signalization criteria defined by the California Manual on Uniform Control Devices (MUTCD) as determined through the data collected with the Annual Traffic Data Collection Contract. Based on MUTCD traffic signal warrant analysis and the City of Visalia traffic signalization procedures, the intersection of Mooney and Ferguson meets the criteria for a traffic signal. Project includes design and construction. (Multi-funded project total of \$800k from \$550K LTF (281) and \$250 Measure R Local (131).)	Casey Chu	PWC103	\$	B22	275,000	275,000				
2	Greenway Trail - Mill Creek to St. John's: Construct a 10' - 12' wide trail from the Mill Creek Bridge to the St John's Trail. Includes landscaping with native trees and shrubs following City of Visalia Urban forestry standards, installation of solar lighting, benches, trash receptacles and trail signage along the trail. This project is Phase 3 of the Greenway Trail and will complete the third (3rd) segment of the planned four (4) segments to construct the Greenway Trail from Walnut Ave. to the St. John's Trail. Contingent upon receiving ATP grant funds. Multi funded project total of \$1.83m from \$230k Measure R Trailways and \$1.6m from State Transportation (281-ATP).	Diego Corvera	PWCP71	\$	B7		1,600,000				

Attachment A: Summary of Proposed Capital Projects by Fund

State Transportation - 281 (2810) - Continued 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
3	Visalia Parkway Improvements-North Half: Extend Visalia Parkway between Stonebrook Street and Court Street. Visalia Parkway is identified as an arterial transportation corridor in the Circulation Element of the City's General Plan. Construct the northerly half to provide one lane of travel in each direction within current right of way. Improves mobility and connectivity of the southern shopping district and neighborhoods in the south east community of the City. Future CIP, Visalia Parkway & Santa Fe Intersection, will connect Visalia Parkway from Court to Santa Fe, and establish future alignment east towards Ben Maddox. Multi funded project total of \$3.55m from \$2.08m Transportation Impact (241) and \$1.459m State Transportation (281-LTF).	Casey Chu	PWCP82	\$	D11			1,459,000			
4	Shirk and Ferguson Traffic Signal: Install traffic signal at Shirk and Ferguson. The current all-way stop intersection meets the traffic signalization criteria defined by the California Manual on Uniform Control Devices (MUTCD) as determined through the data collected with the Annual Traffic Data Collection Contract. Based on MUTCD traffic signal warrant analysis and the City of Visalia traffic signalization procedures, the intersection of Shirk and Ferguson meets the criteria for a traffic signal. Project includes design and construction. (Multi-funded project total of \$800k from \$275K LTF (281) and \$525K Measure R Local (131).)	Casey Chu	PWC102	\$	A15			275,000			
5	Riggin Ave Signal Interconnect: Install communication conduit and fiber optic cable in Riggin Avenue from American Street to Kelsey Street, interconnecting the two traffic signals to provide real time information on the operations of each traffic signal without having to send a signal technician to each intersection for an inspection. Includes installation of traffic monitoring cameras. Project will connect to the communication conduit and cable previously installed in Plaza Drive. Alignment of the communication infrastructure is in accordance with the City's adopted Intelligent Transportation System (ITS) Strategic Plan for expansion of the City's ITS network. Contingent upon receiving grant funds from State and/or Federal funds.	Eric Bons	PWCP29	--	A13			150,000	660,000		

Attachment A: Summary of Proposed Capital Projects by Fund

State Transportation - 281 (2810) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
6	Signal Interconnect and Sanitary Lift Station Connectivity: Provide a City owned fiber optic connection to existing sanitary lift stations near airport terminal building and sanitary lift station at north side of golf course along south side of Hwy198 and extend signal interconnect to Crowley Ave at Plaza Drive. Involves utilization of existing communication conduit and installation of new conduit from Airport Administration Building to each of two sanitary lift stations. Equips each sanitary lift station for remote monitoring of sanitary flows. Contingent upon receiving state/federal grant funds. Multi-funded total of \$1.6m from \$430k WWTP (431) and \$1.2m Fed/State Grant (281). Project will not move forward without grant funding.	Eric Bons	PWCP62	--	C7				1,160,000		
7	Traffic Control Signal Installation: Construct one traffic signal per year as identified by the data collected with the Annual Traffic Data Collection contract. The data will be analyzed to determine which intersections meet the traffic signal criteria as defined by the California Manual on Uniform Traffic Control Devices. Future funds will address one intersection per year. Based on MUTCD warrant analysis and City of Visalia ranking systems. Multi-funded annual project from LTF (281) and Measure R Local (131).	Casey Chu	CP0038	\$	N/A				275,000	275,000	275,000
8	Plaza Drive Signal Interconnect: Install communication conduit and fiber optic communication cable in Plaza Drive from Crowley Avenue to Riggins Avenue to interconnect the five traffic signals. Includes installation of traffic monitoring cameras. Project will connect to the communication conduit and cable installed by the proposed project to provide communications to the sanitary sewer lift station within the golf course and the associated signal interconnect. The alignment of the communication infrastructure is in accordance with the City's adopted Intelligent Transportation System Strategic (ITS) Plan for expansion of the ITS network. Contingent upon receiving grant funds from State and/or Federal funds.	Eric Bons	PWCP28	--	A14				220,000	1,400,000	

Attachment A: Summary of Proposed Capital Projects by Fund

State Transportation - 281 (2810) - *Continued* 2022/23 - 2027/28 Capital Improvement Program

#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
9	American Street Extension Goshen to Riggins: Includes curb & gutter, sidewalks, curb ramps and extension of street. Project requires award of an EDA grant or cost sharing with the County to move forward. FY 24/25 (Staff submit grant application), FY 25/26 (Design and ROW), FY 26/27 - FY 27/28 (Construction). Improves mobility of traffic for the industrial park and the surrounding businesses while reducing congestion and increase the level of service for the roadway. Multi funded project total of \$7.4m from \$1.2m Transportation Impact (241) and \$6.2m EDA grant (281). Project will not move forward without grant funding.	Chantha Chap	CP0309	\$\$	A12					3,107,200	3,107,200
						275,000	1,875,000	1,884,000	2,315,000	4,782,200	3,382,200

Notes: Multi-funded means this is only this fund's portion of the total amount budgeted. These projects are funded from multiple sources and the full budget is shown in the "Multi-Fund 301".

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- - No Annual Maintenance Costs (or no increase over existing cost)

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\$ Annual Maintenance cost is \$5,000 or less

\$\$\$\$

Project will result in savings as described in project description

\$\$ Annual Maintenance costs is \$5,000 to \$25,000

Directory of Map Points

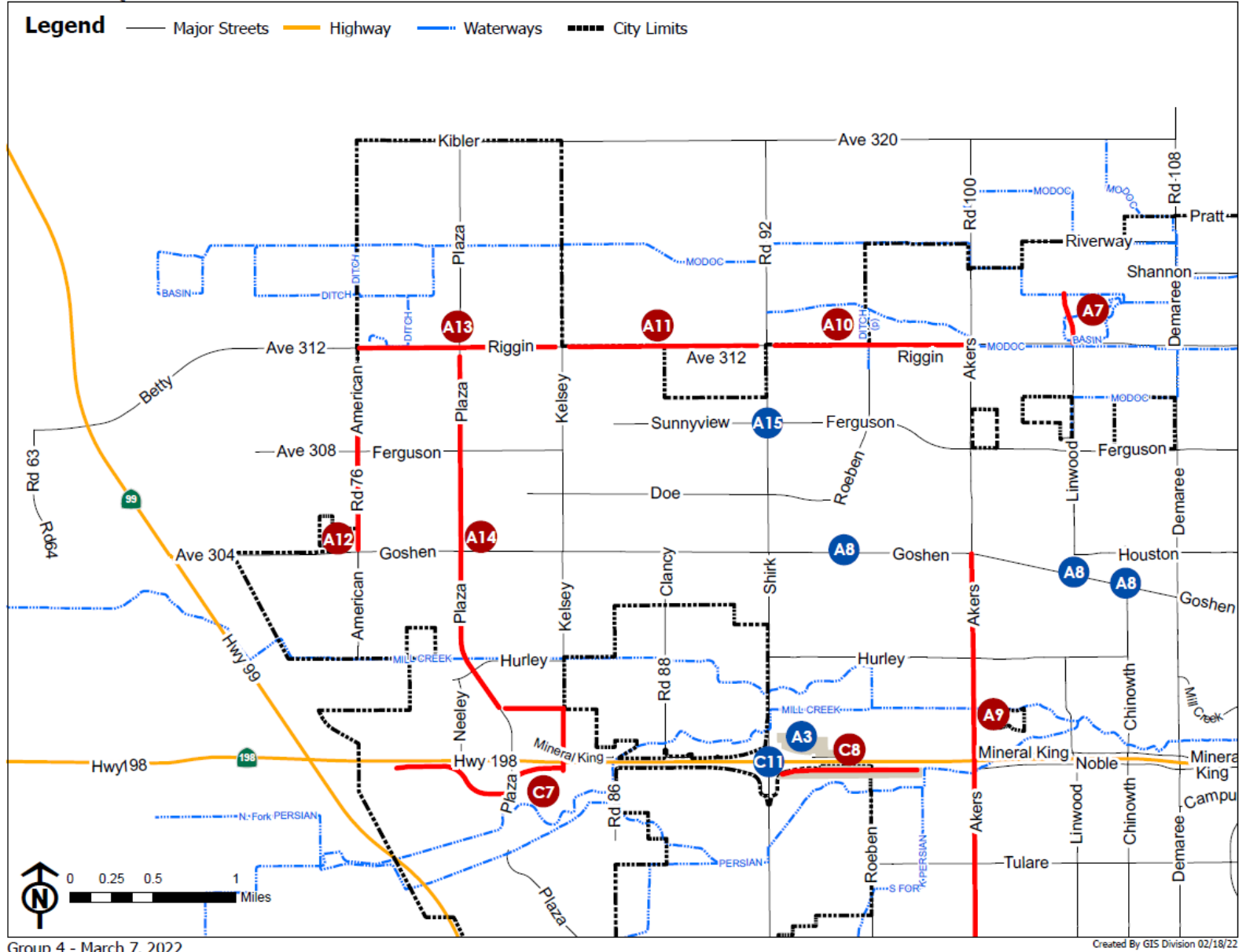
Map Point	Project/Description	Fund	Fund Description
A3	Northside Scenic Corridor - Hillsdale Park/Storm Basin	241	Transportation Impact Fees
A7	Linwod Street Embankment	241	Transportation Impact Fees
A8	NW Quadrant At-Grade Railroad Crossing Study Location	131 241	Measure R Local Transportation Impact Fees
A9	Akers Street Traffic Signal Interconnect	131	Measure R Local
A10	Riggen Widening Shirk to Akers	133 241	Measure R Regional Transportation Impact Fees
A11	Riggin Widening - Kelsey to Shirk	241	Transportation Impact Fees
A12	American Street Extension Goshen to Riggin	241 281	Transportation Impact Fees State Transportation
A13	Riggin Avenue Signal Interconnect American to Kelsey	281	State Transportation
A14	Plaza Drive Signal Interconnect Crowley Avenue to Riggin Avenue	281	State Transportation
A15	Shirk and Ferguson Traffic Signal	131 281	Measure R Local State Transportation
B7	Greenway Trail - Mill Creek to St. John's	281	State Transportation
B8	Tower Street - Mineral King to St John's Parkway	241	Transportation Impact Fees
B13	Main-Center One-Way Conversion	111 131	Gas Tax Measure R Local
B14	Pedestrian Ramps and Intersection Improvements	111	Gas Tax
B15	Oak Avenue Extension from Tipton Street to Burke Street	131	Measure R Local
B16	Traffic Signal Modification at Giddings and Mineral King	131	Measure R Local
B17	Goshen Widening Ben Maddox to Burke	131 241	Measure R Local Transportation Impact Fees
B18	Construct Santa Fe Street Houston to Riggin	241	Transportation Impact Fees
	Santa Fe Environmenal Cleanup	241	Transportation Impact Fees
	Santa Fe Street - Houston to Riggin (Design)	241	Transportation Impact Fees
B19	Ben Maddox Widening from Center Avenue to Goshen Avenue	241	Transportation Impact Fees
B20	Riggin Widening from Mooney to Conyer	241	Transportation Impact Fees
B21	Houston - Santa Fe Roundabout Landscaping	241	Transportation Impact Fees
B22	Mooney and Ferguson Traffic Signal	131 281	Measure R Local State Transportation

Directory of Map Points (Continued)

Map Point	Project/Description	Fund	Fund Description
C4	Walnut Avenue Communications Connectivity	131	Measure R Local
C7	Signal Interconnect and Sanitary Lift Station Connectivity	131 281	Measure R Local State Transportation
C8	Southside Scenic Corridor	241	Transportation Impact Fees
C9	Traffic Signal Modification at Chinowth and Whitendale	111	Gas Tax
C11	Shirk/SR 198 Interchange (Long-Term Improvements)	133	Measure R Regional
C12	Shirk Street and Walnut Avenue Roadway Improvements	131 241	Measure R Local Transportation Impact Fees
D6	Lovers Lane Center Median from Cherry to K Avenue	131	Measure R Local
D7	Traffic Signal Modifications at Lovers Lane and Walnut Ave	131 241	Measure R Local Transportation Impact Fees
D8	Caldwell Signal Interconnect Fiber Optic Santa Fe to Lovers Lane	131	Measure R Local
D9	Design Tower Street from Walnut Ave to Visalia Parkway	131 241	Measure R Local Transportation Impact Fees
D10	Design and Construct Tower St from Cypress to Walnut	131 241	Measure R Local Transportation Impact Fees
D11	Visalia Parkway Improvements - North Half	241 281	Transportation Impact Fees State Transportation
D12	Visalia Parkway and Santa Fe Intersection (Design)	241	Transportation Impact Fees
	Visalia Parkway and Santa Fe Intersection (Construction)	241	Transportation Impact Fees
D13	Tulare Avenue Extension to Tower Road	241	Transportation Impact Fees
D14	Laura Avenue Widening at Garden Street	131	Measure R Local
D15	Design-Major Street Rehab	131	Measure R Local
D16	Design - Major Street Rehab - Giddings Street	131	Measure R Local

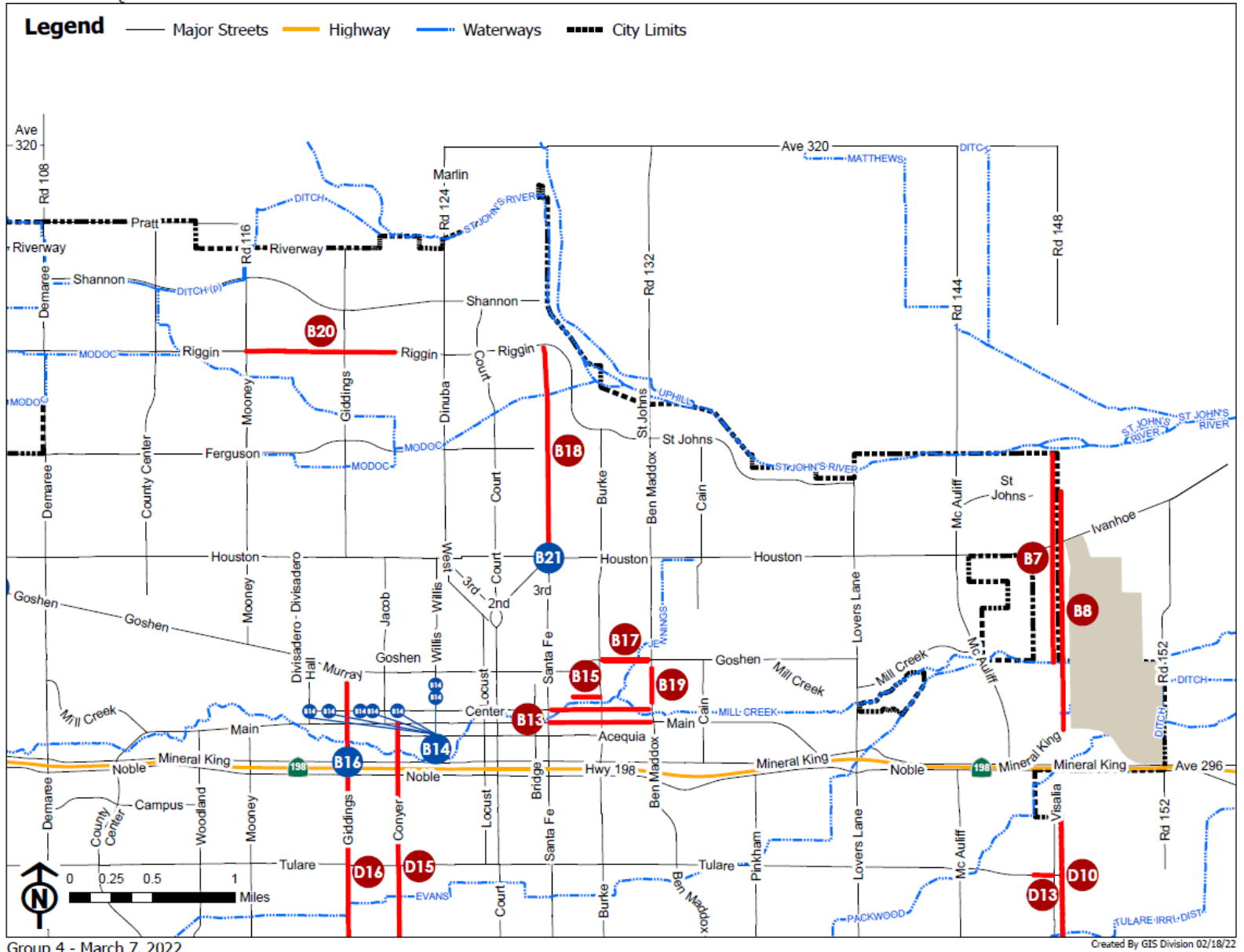
Attachment B: Directory of Map Points and Maps

CIP BUDGET - QUAD A



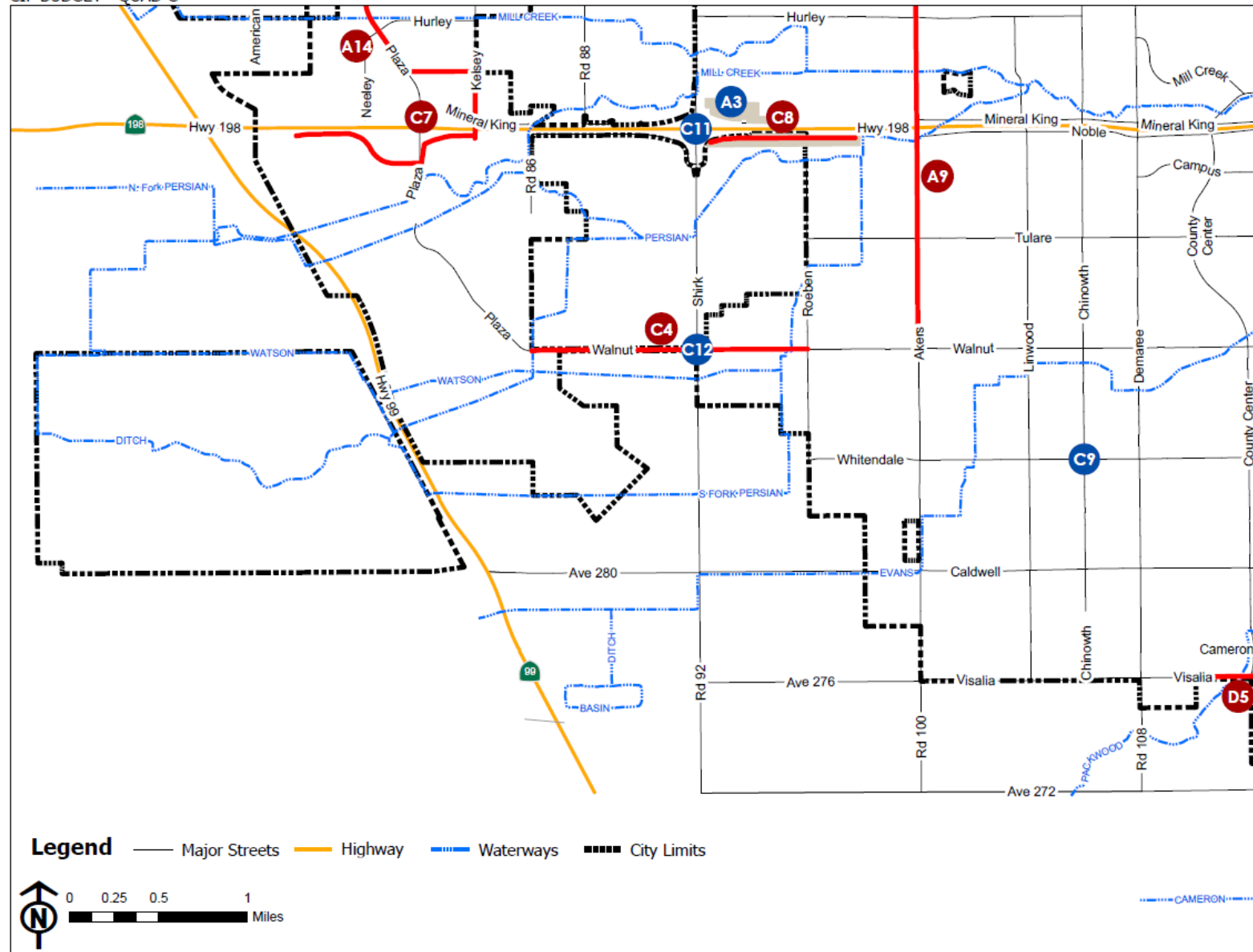
Attachment B: Directory of Map Points and Maps

CIP BUDGET - QUAD B



Attachment B: Directory of Map Points and Maps

CIP BUDGET - QUAD C

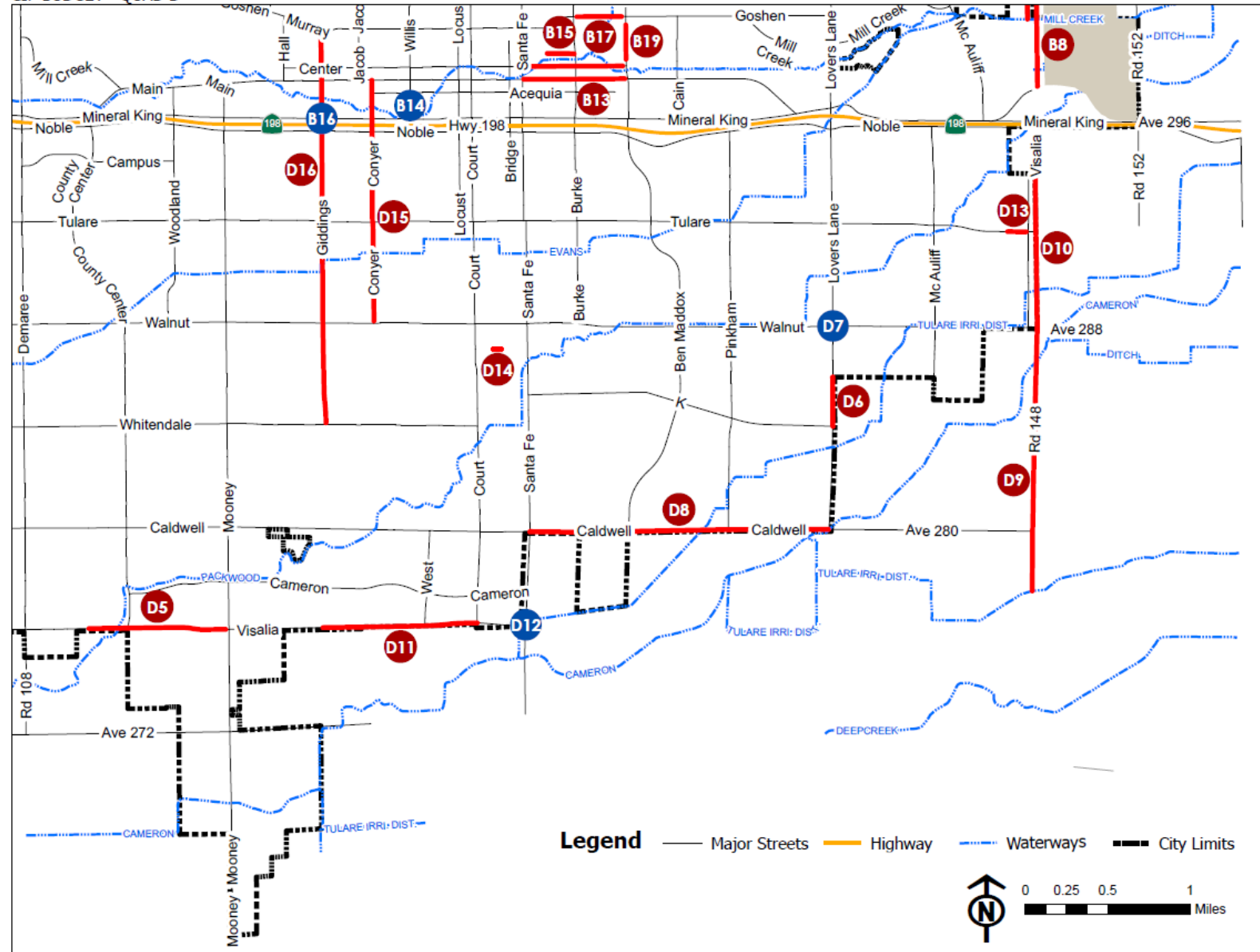


Group 4 - March 7, 2022

Created By GIS Division 02/18/22

Attachment B: Directory of Map Points and Maps

CIP BUDGET - QUAD D



Group 4 - March 7, 2022

Created By GIS Division 02/18/22



City of Visalia

Capital Budget 2022-2028

Proposed CIP Adoption Process

- No Action required tonight
- Review at (6) February-March meetings
- Staff Available for questions
- Bring back if necessary
- Final Adoption of All Funds in June

Budget Structure

- Cash Balance Summary
- Project Descriptions
- Map References
- 6 Year Plan
- 2-year Proposed Budget
- 4-year Planning Purposes

Recreation Facilities - 211 (2440) 2022/23 - 2027/28 Capital Improvement Program											
This fund is derived from impact fees paid by developers in lieu of providing parks and open space. Funds are to be used to implement the Parks Master Plan for open space acquisition and providing park and other recreational facilities.											
Cash Balance Summary											
	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28					
Beginning Cash Balance	5,900,400	4,189,000	3,614,100	(5,138,600)	(9,676,800)	(28,065,200)					
Park and Recreational Impact Fees	1,960,000	1,999,200	2,039,200	2,080,000	2,121,600	2,164,000					
Interest Earnings	20,000	20,000	18,100	(25,700)	(48,400)	(140,300)					
Operating Expenditures	(21,400)	(21,400)	(21,800)	(22,200)	(22,800)	(23,100)					
Capital Improvements	(3,670,000)	(2,972,700)	(10,788,200)	(6,570,300)	(20,439,000)	(16,420,800)					
Total Resources Available for Future Projects	4,189,000	3,614,100	(5,138,600)	(9,676,800)	(28,065,200)	(42,485,200)					
#	Project Description (100 word limit)	Project Manager	Project # (or "new")	Budget Impact	Map Ref	2022-23	2023-24	2024-25	2025-26	2026-27	2027-28
1	Neighborhood Park: Develop a specific plan to acquire land, design, and build a neighborhood park of 3-5 acres in the northwest quadrant of the City located in accordance with the General Plan's element on Park and Recreation Facilities and development trends. Park improvements to include walking paths, playground, picnic area, and open turf. Funding for design to begin in 2022-23 with construction to follow in 2023-24. Annual maintenance costs for this park are estimated to be \$74k per year.	Leslie Caviglia	CP0425	\$\$\$	N/A	1,540,000					
2	3-Acre Neighborhood Park: Per the general plan the City will acquire land, design, and build a 3-acre neighborhood park in the northeast portion of the City. Neighborhood Parks are intended to provide basic recreation activities for one or more neighborhoods with a service area of one-half to one mile radius. Neighborhood parks usually include playgrounds, picnic tables, benches, and walkways. This location will be designed to include a Splash Pad. Annual maintenance costs for this park are estimated to be \$74k per year.	Jeremy Rogers	CSCP53	\$\$\$	N/A	1,200,000	1,275,000				
3	Cameron Creek Regional Park/Basin: Acquire site for a 5.25 acre storm/recharge park, 4 acre park, and half mile asphalt trail along southerly extension of McAuliff adjacent to Cameron Creek. To serve existing and future development within the one-mile development block of Lovers Lane to Road 148 and Caldwell to Walnut. May also receive TID exchange water from Cameron Creek, through development of a later project. Geotechnical testing will be required to determine eligibility for ground water recharge funding. (Multi-funded: Project total \$2.7m from \$1.6m Parks & Rec (211), \$558K Storm Sewer (221) and \$558K GW Recharge (224)). Annual maintenance costs for this park are estimated to be \$116k per year.	Cody McLaughlin	PWCP72	\$\$\$	D4	537,000		1,136,900			

Funds for review tonight

Fund Name	Fund #	Proposed 2022-23	Proposed 2023-24
Gas Tax	111	2,679,800	2,615,000
SB1-RMRA	113	4,100,000	3,300,000
Measure R Local	131	2,419,800	3,892,000
Measure R Regional	133	-	-
Transportation Impact Fees	241	6,979,200	5,239,300
Landscape & Lighting	273	252,000	766,000
State Transportation	281	275,000	1,875,000
Total Projects		\$ 16,705,800	\$ 17,687,300

Gas Tax Fund - 111 (2370)

#	Project Description	Proposed 2022-23	Proposed 2023-24
<i>New Construction, Improvements and Administration:</i>			
1	Roadway Preliminary Engineering	\$ 75,000	\$ 75,000
2	Developer Reimbursements	75,000	75,000
3	Traffic Signal Modification at Chinowth and Whitendale	50,000	125,000
4	Traffic Counts, Survey, and Supplemental Services	50,000	55,000
5	Bike Plan Implementation	\$ 50,000	50,000
6	Upgrade Traffic Signal Controllers		\$ 100,000

Gas Tax Fund - 111 (2370) - *Continued*

#	Project Description	Proposed 2022-23	Proposed 2023-24
Street Maintenance:			
9	Thin Skin Patching	\$ 700,000	\$ 700,000
10	Crack Seal	350,000	350,000
11	Reclamite	200,000	200,000
12	Annual Striping Contract	160,000	160,000
13	Dig Outs	100,000	100,000
14	Minor Asphalt Overlays	100,000	100,000
15	Pot Hole Patching	100,000	100,000
16	Cape Seal	75,000	75,000
17	Pave Outs	75,000	75,000
18	Replace Traffic Signs	75,000	75,000
19	Replace Street Name Blades	\$ 40,000	\$ 40,000

Gas Tax Fund - 111 (2370) - *Continued*

#	Project Description	Proposed 2022-23	Proposed 2023-24
<i>Street Maintenance (Continued):</i>			
20	Battery Backup Maintenance	\$ 20,000	\$ 20,000
21	Hot Patching	20,000	20,000
22	Traffic Signal Maintenance	10,000	90,000
23	Neighborhood Traffic Calming		30,000
<i>Streets Vehicle and Equipment Replacements:</i>			
25	Replace Ingersol Rand Vibratory Compaction Roller (Unit is 22 years old)	180,400	
26	Replace Crack Filler (Unit is 22 years old)	87,200	
27	Replace Trailer Mounted Oil Distributor (Unit is 28 years old)	87,200	
	Total Expenses	\$ 2,679,800	\$ 2,615,000

SB1-Road Maintenance and Rehabilitation Act - 113

#	Project Description	Proposed 2022-23	Proposed 2023-24
1	Pavement Rehabilitation - SB1	\$ 4,100,000	\$ 3,300,000
	Total Expenses	\$ 4,100,000	\$ 3,300,000

Measure R Local Fund - 131 (2410)

#	Project Description	Proposed 2022-23	Proposed 2023-24
1	Install Traffic Monitoring Cameras	\$ 370,000	
2	Design - Major Street Rehab - Conyer St	300,000	
3	Lovers Lane Center Median from Cherry to K Ave	265,000	
4	Miscellaneous Sidewalk Improvements	240,000	
5	Update Improvement Standards	228,000	
6	Vehicle Miles Traveled (VMT) Mitigation Nexus Study	220,000	
7	Local Roads Safety Plan (LRSP)	190,000	
8	Oak Avenue Extension	181,600	\$ 1,284,500
9	ADA Self Evaluation and Transition Plan (City Right-of-Way)	150,200	
10	Traffic Management Center (TMC) Software Support	100,000	
11	Active Transportation Plan Update	100,000	
12	Miscellaneous Pedestrian Improvements	25,000	120,000
13	Traffic Signal Optimization Project (TSOP)	25,000	35,000
14	Stop Sign Installations	\$ 25,000	\$ 25,000

Measure R Local Fund - 131 (2410) - *Continued*

#	Project Description	Proposed 2022-23	Proposed 2023-24
15	Shirk St. & Walnut Ave Roadway Improvements		\$ 780,000
16	Design - Major Street Rehab - Giddings St		300,000
17	Mooney & Ferguson Traffic Signal		250,000
18	Accessibility Upgrades & Barrier Removal		218,800
19	NW Quadrant At-Grade Railroad Crossing		200,000
20	Battery Backup System Installation Traffic Signals		150,000
21	Traffic Signal Modifications at Lovers Lane and Walnut Ave		150,000
22	Caldwell Signal Interconnect Fiber Optic (SICFO) Santa Fe to Lovers Lane		125,000
23	Shirk and Ferguson Traffic Signal		100,000
24	Design Tower St (Rd 148) from Walnut Ave to Visalia Parkway (1.6 miles)		92,100
25	Design and Construct Tower St (Rd 148) from Cypress Ave to Walnut Ave (4200 feet)		61,600
	Total Expenses	\$ 2,419,800	\$ 3,892,000

Transportation Impact Fees Fund - 241 (2520)

#	Project Description	Proposed 2022-23	Proposed 2023-24
1	Riggin Widening - Kelsey to Shirk	\$ 2,375,000	
2	Santa Fe Environmental Cleanup	1,886,200	
3	Developer Reimbursements	1,100,000	\$ 1,100,000
4	Visalia Parkway Improvements-North Half	631,400	
5	Riggin Widening from Mooney to Conyer	454,000	
6	Traffic Signal Modifications at Lovers Lane and Walnut Ave	160,000	445,000
7	Houston-Santa Fe Roundabout Landscaping	157,600	
8	Linwood Street Embankment	113,000	254,500
9	TIF Adminstration	100,000	\$ 10,000
10	East Side Regional Park EIR	\$ 2,000	

Transportation Impact Fees Fund - 241 (2520) - *Continued*

#	Project Description	Proposed 2022-23	Proposed 2023-24
11	Design Tower St (Rd 148) from Walnut Ave to Visalia Parkway (1.6 miles)		\$ 92,000
12	Design and Construct Tower St (Rd 148) from Cypress Ave to Walnut Ave (4200 feet)		79,300
13	Shirk St. & Walnut Ave Roadway Improvements		1,820,000
14	Visalia Parkway & Santa Fe Intersection (Design)		855,000
15	NW Quadrant At-Grade Railroad Crossing		486,800
16	Tower Street-Mineral King to St. John's Parkway (Prelim Design)		96,700
	Total Expenses	\$ 6,979,200	\$ 5,239,300

Special Service District-Landscape and Lighting - 273 (2730)

#	Project Description	Proposed 2022-23	Proposed 2023-24
1	Reclamite Landscape & Lighting Districts	\$ 252,000	\$ 568,000
2	Crack Seal in Landscape and Lighting Districts		198,000
	Total Expenses	\$ 252,000	\$ 766,000

Local Transportation Fund - 281 (2810)

#	Project Description	Proposed 2022-23	Proposed 2023-24
1	Mooney & Ferguson Traffic Signal	\$ 275,000	\$ 275,000
2	Greenway Trail - Mill Creek to St. John's		1,600,000
	Total Expenses	\$ 275,000	\$ 1,875,000

Next meeting



Staff will return with any requested information or revisions at next meeting, March 14th



City of Visalia

Staff Report

Visalia City Council
707 W. Acequia
Visalia, CA 93291

File #: 22-0097

Agenda Date: 3/7/2022

Agenda #: 1.

Agenda Item Wording:

Authorization to read ordinances by title only.



City of Visalia

Staff Report

Visalia City Council
707 W. Acequia
Visalia, CA 93291

File #: 22-0044

Agenda Date: 3/7/2022

Agenda #: 2.

Agenda Item Wording:

Authorization to direct staff to appropriate American Rescue Plan Act (ARPA) funding from the Federal Transit Administration (FTA) to the Transit Operations budget (4551), and to approve allocation requests up to \$1,827,167.

Deadline for Action: None

Submitting Department: Administration - Transit

Contact Name and Phone Number:

Angelina Soper, Transit Manager Angelina.Soper@Visalia.City (559) 713-4591

Nick Mascia, Assistant City Manager Nick.Mascia@Visalia.City (559) 713-4323

Department Recommendation:

Direct staff to appropriate American Rescue Plan Act (ARPA) funding from the Federal Transit Administration (FTA) to the Transit Operations budget (4551) in the amount of \$1,827,167 and to submit allocation requests for operations costs up to \$1,827,167 until funds are completely exhausted.

Background Discussion:

In the Fall of 2021, the Federal Transit Administration (FTA) announced the American Rescue Plan Act's (ARPA) Additional assistance funding to assist with costs related to COVID-19. As a result of the COVID-19 pandemic, Visalia Transit was allocated these additional federal funds on top of our regular operations allocations from the FTA to supplement the City's operational costs and assist with the unexpected expenses and revenue losses. These funds were apportioned through the regular 5307 operations assistant grant to Visalia based on the Urbanized area formula. These funds provided 132% of 2018 operating expenses when combined with previous CARES Act and CRRSAA funds. Expenses of \$1,807,147 have already been incurred with a balance of \$20,020 available for additional operational expenses.

Transit staff brought the Coronavirus Aid, Relief and Economic Security Act (CARES) funding as an informational item to Council back in May 2020 and Council authorized the appropriation of funds in July 2020. American Rescue Plan Act (ARPA) funds are in addition to the CARES funds previously awarded. Transit will use these funds for the following:

- **Cover all operating and COVID-19 related expenses**
 - **Additional staffing expenses for cleaning and sanitizing**
 - **Additional safety equipment and products for staff, and drivers**
- **Supplement lost revenues due to COVID-19**

Fiscal Impact:

These funds have been used to supplement the Transit operations budget.

Prior Council Action:

July 20th, 2020 Council approval of CARES funding in the amount of \$13,048,019

Alternatives: N/A

Recommended Motion (and Alternative Motions if expected):

I move to authorize staff to appropriate ARPA funding in the amount of \$1,827,167 to the Transit Operations Budget from the FTA and to approve the allocation requests up to \$1,827,167 until funds are exhausted.

Environmental Assessment Status: N/A

CEQA Review: N/A

Attachments: None



City of Visalia

Visalia City Council
707 W. Acequia
Visalia, CA 93291

Staff Report

File #: 22-0069

Agenda Date: 3/7/2022

Agenda #: 3.

Agenda Item Wording:

Authorize staff to accept and appropriate funding for fire department training projects that will be reimbursed through the California Firefighter Joint Apprenticeship Committee (Cal-JAC) account in the amount of \$94,770.

Deadline for Action: 3/7/2022

Submitting Department: Fire Department

Contact Name and Phone Number: Nick Branch x4545; Crissy Balderama x4513; Tom Van Grouw x4265

Department Recommendation:

It is recommended that the City Council authorize the Visalia Fire Department to utilize funding from the California Joint Apprentice Committee account to purchase items for fire department training in the amount of \$94,770.

Background Discussion:

In 2006, the Visalia Fire Department became a participating department of the California Joint Apprenticeship Committee (Cal-JAC) apprentice agreement. The California Joint Apprenticeship Committee is co-sponsored by the Office of the State Fire Marshal and the California Professional Firefighters. Cal-JAC provides funding to participating California fire departments by awarding funding based upon eligible submitted training hours each month for firefighters under apprentice agreements. Firefighters participating in the program for apprenticeship in their assigned positions are awarded \$3.20 per hour into an account allocated for Fire Department training expenses. This amount changes annually and is based on state allocations to the program. The account accumulates over time and the funding can be utilized with approval of the Fire Department Cal-JAC committee. Per Cal-JAC rules and regulations, the Cal-JAC committee is comprised of members from both fire administration and labor. The Visalia Fire Department has used little of the awarded funding over the last several years and is now requesting to purchase the items included in this transmittal. If approved, all purchases will be made following the City of Visalia purchasing policy.

This specific purchase would be used to provide the following items to fire department training operations:

- Fire Vent Variable pitch roof prop that will replace an existing, wood-framed prop that is deteriorating and will soon be unsafe for training operations. This Fire Vent roof prop will provide firefighters with the ability to train on roof operations such as ventilation tactics, smoke removal, aerial operations, and will also provide operations for both low and steep angle rope rescue. It is constructed of a steel frame and will provide decades of good training opportunities. The Fire Vent roof prop manufacturer has sole proprietary

manufacturing rights and patents to this product and is the only distributor for this type of training prop. Cost: \$76,500

- Forcible entry training prop that will be an addition to a door prop purchased in 2011 with CalJAC funding. This additional door prop will allow for simultaneous training to take place in different locations on the training grounds. This prop will provide training on both forcible entry and less destructive techniques to enter a locked structure. It also provides props for locked gates and chained access props. East Coast Rescue Solutions also has proprietary rights to their training prop and provides safety and warranty items that are not incorporated into other forcible entry training props. Cost: \$12,580.
- Rogue Cardio equipment for the training facility, to be utilized year-round by assigned crews and during recruit academies. This purchase will provide three Rogue Echo stationary bikes and three Rogue Concept 2 rowers that will provide low impact endurance and cardiovascular exercise for the firefighters. Cost: \$5,690

Fiscal Impact:

Staff is requesting to appropriate \$94,770 for the purchase of a training roof prop, a forcible entry door/cutting prop, and 6 pieces of exercise equipment for the training center. All expenses will be reimbursed by the California Firefighter Joint Apprentice Committee (Cal-JAC) account.

Prior Council Action: N/A

Other: N/A

Alternatives: Purchase items through the appropriated fire department training budget over the next several budget years, excluding the ventilation roof prop which is outside the allocated budget amount.

Recommended Motion (and Alternative Motions if expected):

I move to authorize staff to appropriate funding from the general fund in the amount of \$94,756.31 for the sole source purchase of one Fire Vent roof prop in the amount of \$76,493.50, the sole source purchase of one forcible entry prop in the amount of \$12,580.58 from East Coast Rescue Solutions, and six pieces of exercise equipment from the awarded bidder Rogue Fitness in the amount of \$5,682.23. All costs will be reimbursed by California Firefighter Joint Apprenticeship Committee.

Environmental Assessment Status: N/A

CEQA Review: N/A

Attachments: 1. Fire Vent Prop Brochure - Sole Source - Quote. 2. East Coast Rescue Solutions Forcible Entry Brochure - Sole Source - Quote. 3. Rogue Echo Bike and Concept 2 Rower Quote



FireFighting Training Equipment



US PATENTS # 8.360.782, # 9.646.515, # 10.242.594

The Hands-On Training You Need...Where You Need It.



www.FireVent.us

The Hands-On Training You Need...Where You Need It.

Extremely mobile, versatile and affordable. Designed to give firefighters valuable hands on training while remaining in service at their station. Numerous single engine or truck company evolutions. This heavy duty, rugged and self-contained unit is ideal for individual departments or to be shared by multiple agencies.



- Variable Pitch, Vertical Ventilation,
- Gable End Ventilation (Elevated Vertical Wall).
- Forcible Entry
- Overhead Door Forcible Entry/Cutting,
- Confined Space Entry.
- Activated Sprinkler Heads With Fire Department Connection.
- Salvage & Overhaul.
- Haz-Mat Dome Leak Simulator.
- Through The Floor / Basement Rescue.
- Ceiling (Sheet Rock) Pulling / Breaching.
- Second Story Window Rescue and Ladder Evolutions.
- Firefighter Bailout Window.
- Many Other Possible Training Evolutions



Mobile • Versatile • Affordable



Portable Units



FireVent's Portable Units are designed to be easy to set up and take down and move from location to location. These units are able to go from a flat position to a 12/12 pitch. There are currently two sizes available; 8ft x 12ft and 10ft x 16ft.

- Completely Portable, Easy To Set Up And Take Down.
- Use Outside Or Inside, At Individual Stations Or The Training Center.
- Two Sizes Currently Available: 8ft x 12ft & 10ft x 16ft.
- Able To Go From Flat To 12/12 pitch.
- All Tubular Steel Construction.
- All Components Are Powder Coated,
- Patent Pending





5998 MORGAN MILL RD CARSON CITY, NV 89701

P: 775.230.9953 F: 775.883.2387

info@FireVent.us

PRODUCT INFORMATION



Mobile Units

Our mobile units have a very unique, innovative and fully patented design that allows the props to open and close, from the traveling position to the training position, with the use of two heavy-duty hydraulic cylinders. Mobile FireVent units are much more than just ventilation props; they are multi-discipline training units. There are three base model units that can be customized and configured with numerous options to meet your department's needs



Stationary Units

Our stationary units are an excellent addition to a planned or established training center. Each stationary unit is extremely heavy duty and well made. They are custom-built at the time of order with the same high-quality materials and craftsmanship as the mobile units. The stationary units are designed and engineered in a modular fashion to enable ease of shipping and erecting on site. Stationary units can be placed and secured on a slab or footing style foundation



Auto-X Simulator

The Auto-X Simulator is excellent for entry-level learning (academy setting) or first-time exposure to the extrication tools (hydraulic spreaders, cutters, ram and sawzall, rotary saw, and rescue strut). The Auto-X allows for the safe use and operation of the tools in a controlled environment, with predictable results, and enables high repetition (lots of tool time) for students to gain confidence and understanding of the tools, making time spent with acquired vehicles far more productive.





To: Whom it may concern
From: FireVent, LLC.
Subject: **Sole Source Provider**
Date: 01/22/22

The purpose of this letter is to state that FireVent, LLC is the designer and manufacturer of the FireVent Mobile Training Units and the FireVent Auto-X Simulator. FireVent, LLC has sole proprietary manufacturing rights. These FireVent products are patented and protected under United States Patent Numbers 8,360,782 & 9,646,515 & 10,242,594. FireVent, LLC is also the designer and manufacturer of the FireVent Portable Unit, which is currently patent pending with the USPTO, and the FireVent Stationary Unit. Any attempt to replicate or reproduce these FireVent products would result in patent infringement. FireVent, LLC is the sole source provider & distributor of these FireVent products. There are no other companies or manufacturers that have any products that resemble the FireVent Mobile Units, Portable Units, Stationary Units or the FireVent Auto-X Simulator in their product line.

FireVent, LLC is also a single source manufacturer.

Thank you,

Rod Temple, GM.
rod@FireVent.us

Price List For:

Stationary Unit and Available Options

FireVent, LLC.
3031 N. Deer Run Rd.
Carson City, NV 89701
Phone: (775) 230-9953 Fax: (775) 883-2387

Date: 2021

[illegible]

Notes:

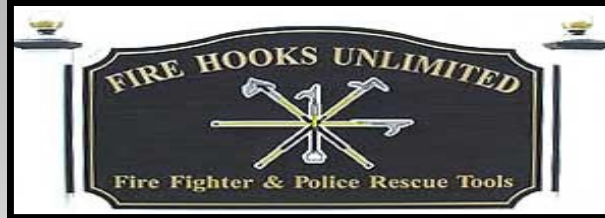
Door Features

- Inward/Outward Forces
- Three locks for varying degrees of difficulty.
- 1/2" Heavy Security rebar lock
- Engineered steel door allows for flex of door when forcing.
- Ability to force drop bars and cut or pike carriage bolts.
- Thru-the Lock, hinge pulling and cutting, rebar cutting ,padlock cutting and breaking props that mount to the side of the door prop.
- Fixed brackets for mounting walls for restricted space.
- Replaceable door sleeve and stop for added longevity
- Removable door dolly system.
- Door prop can also be easily disassembled for transport.

VISIT

eastcoastrescuesolutions.com to shop our complete line of Fire/Rescue equipment.

Enter FDIC2021 for 10% off
Offer Expires August 31, 2021



Instagram

Contact Us For More Information

chris.ecrs@gmail.com

917-886-2579

www.eastcoastrescuesolutions.com

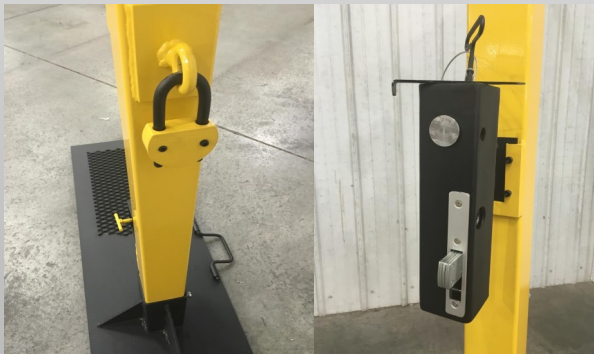
East Coast **RESCUE** Solutions

Forcible Entry Simulators



All-In One Props

TTL,Hinge,Rebar



Forcible Entry

Drop Bar



Forcible Entry

Metal Cutting





East Coast Rescue Solutions leads the industry in the design and manufacturing of Forcible Entry Simulators. ECRS forcible entry doors can be used for training your members on forcing inward/outward swinging doors and are extremely durable, being used at FDIC, Firehouse Expo, HROC, MAFFC and hundreds of fire department and training academies across the country for over 10 Years!

- Our full-length door stop allows you to force the door top to bottom giving you over 65" of workable door stop.
- We have engineered our door to flex from top to bottom depending on lock location just as a real metal door would do.
- Our Door can be shocked from top to bottom – **No simulation or restrictions**
- Our Doors Have **THREE** different locking areas(Top, Middle, Bottom) which allows your firefighters to challenge themselves from basic deadbolts to heavy security scenarios that they could face on the fireground
- For added resistance Drop Bar is added to simulate outward forces found on rear commercial doors. This feature also allows your firefighters to attack carriage bolts with a saw or conventionally
- Doors use 2x2 and/or 1x2 wood blocks for lock resistance
(Wood is easily obtainable at any home improvement store and no templates are needed)
- The Hinge side is adjustable to allow you to vary the door gap and the difficulty of forces **(industry only)**
- Hydra ram (bunny, rabbit tool) can be used in any location on the door
- There is a replaceable sleeve over the door for added strength and allowing shocking of the door
- Our doorstep is also replaceable allowing you to replace a part instead of a new door prop
- The prop can be easily disassembled. A Wheeled Dolly is also available for easy prop relocation.
- We also offer our All-In-One prop options. These props include Thru-the-Lock, Hinge Pulling/Cutting, Rebar Cutting and Padlock Cutting/Breaking props that can be added directly to the door prop. Thus, having all your FE needs on one prop.
- Two-year warranty is included on all doors covering manufacture defect
- **ECRS Doors are the only doors used at FDIC Forcible Entry Classes (nine years) and Firehouse EXPO Forcible Entry Classes**
- **A train the trainer program is recommended with every door purchase**
- We are owned and operated by firefighters from the FDNY and Atlanta Fire Rescue
- ECRS has been producing/selling Forcible Entry Props for over 10 years and are made in the USA.
- Our props come with video links and PDF manuals for proper set-up and operation.



East Coast Rescue Solutions

Forcible Entry Door Simulator – Gen3 Door

RE: Sole Source of the East Coast Rescue Solutions Forcible Entry Door Simulator

This letter is to inform you that East Coast Rescue Solutions Forcible Entry Door Simulators are designed, fabricated, sold, and distributed exclusively by East Coast Rescue Solutions.

Our door simulators offer some industry only features.

- A full length reinforced flexible steel door with drop-bar capabilities gives our prop real world feel with the added feature of durability
- **A full-length replaceable door sleeve.** This feature will save money over buying new door props as a part can be replaced instead of an entire prop.
- **An adjustable door gap.** This function allows instructors to vary the degree of difficulty and duplicate realistic door gaps.
- **Heavy security locks-** Three different locking location along with drop-bar. This will challenge your firefighters as their skill level increases.
- **The ECRS All-In One Prop** -These additional props Includes thru-the-lock, hinge pulling/cutting and rebar cutting prop and padlock cutting prop all can be used at three different heights and mount directly to the Forcible Entry Door. This optional prop can train firefighter in all the basic skills of Forcible Entry. It is the only prop on the market today that can vary the height of each skill.

All East Coast Rescue Solutions Simulators come with a one-year warranty. This warranty covers material and craftsmanship under proper use of the prop.

The East Coast Rescue Solutions Forcible Entry Door is not offered on ANY Private or Public Local, State or Government contracts

Please feel free to contact me with any questions. Thank you

Chris Minichiello/Owner

East Coast Rescue Solutions

917-886-2579

Chris.ecrs@gmail.com

www.eastcoastrescuesolutions.com



East Coast Rescue Solutions

4940 Merrick Road Suite 172
Massapequa Park, NY 11762

Quote

Date	Estimate #
2/2/2022	4532

Name / Address
City of Visalia Fire Department 707 W Acequia Ave Visalia, CA 93291

Ship To
City of Visalia Fire Department 6921 West Furgerson Ave Visalia, CA 93291

P.O. No.	Terms	Due Date	Rep
		2/2/2022	CM

Description	Qty	Rate	Total
East Coast Rescue Solutions Forcible Entry Door Simulator. Door Features: GEN 3 DOOR Inward and outward swinging forces Drop Bar Forcible Entry Cutting or Spiking of Carriage Bolts Full Door Stop 65" Three Locks for varying degrees of difficulty Replaceable Door Sleeve and Jamb for added longevity Fixed bracket to mount walls for restricted space FE Can be disassembled for easy transport. Optional dolly system for easy transport Adjustable door gap All In-One Prop Receivers Professionally powder coated One year warranty Includes Operational Guide and Videos	1	7,900.00	7,900.00
Contact us with any questions. Chris Minichiello 917-886-2579. www.eastcoastreducesolutions.com		Total	



East Coast Rescue Solutions

4940 Merrick Road Suite 172
Massapequa Park, NY 11762

Quote

Date	Estimate #
2/2/2022	4532

Name / Address
City of Visalia Fire Department 707 W Acequia Ave Visalia, CA 93291

Ship To
City of Visalia Fire Department 6921 West Furgerson Ave Visalia, CA 93291

P.O. No.	Terms	Due Date	Rep
		2/2/2022	CM

Description	Qty	Rate	Total
East Coast Rescue Solutions All-In One Props. These props give you the advantage of having all your forcible entry needs on one simulator. With the All-In One props you get our hinge pulling/cutting, rebar cutting station, padlock cutting Prop and the Thru-the-Lock prop that mount right on to our Forcible Entry Door. These props allow you to vary the height of each teaching station so your firefighters can hone their skills with real world applications. Prop Includes: 2 - Hinge Pulling/Cutting Stations - 1 - Rebar Cutting/Burning Stations - 1 - Padlock Cutting Prop with Shackle Bending Jig 1 - Lock Pulling Station (Thru-the-Lock) (no consumables with milled lock cylinder)	1	2,800.00	2,800.00
Drop Ship door to City of Visalia Fire Department	1	895.00	895.00

8.5% tax = \$985.58

Contact us with any questions. Chris Minichiello
917-886-2579.
www.eastcoastrescuesolutions.com

Total \$11,595.00

Total with Tax:
\$12,580.58



545 E. 5TH AVE.
COLUMBUS, OH 43201

(614) 358-6190
TEAM@ROGUEFITNESS.COM

OFFICIAL QUOTE

Bill To:
NICK BRANCH
VISALIA FIRE DEPARTMENT
420 N BURKE ST
VISALIA CA 93292

Ship To:
NICK BRANCH
VISALIA FIRE DEPARTMENT
420 N BURKE ST
VISALIA CA 93292

Total Equipment Cost	\$2,385.00
Shipping (LTL)	\$0.00
Tax	\$202.73
Grand Total	\$2,587.73

Quote #: 112192

Weight	Brand	Item Description	SKU	QTY	Price	Total	Details
148 lbs	Rogue	Rogue ECHO Bike v2.0	IP0822	3	\$795.00	\$2,385.00	

* Shipping quotes are only valid for 24 hours

* All previous versions of this form are obsolete

* Please ensure the items and quantities on this quote are correct prior to placing your order

* Custom products require review and approval by the Rogue creative team and may require modifications to be manufactured.

* 100% Due Upon Order unless otherwise Agreed Upon

* All POs are processed with Net 30 terms starting the date the order ships. POs over \$25,000 will require a 50 % deposit to initiate the order. The remaining 50 % of the balance will have Net 30 terms ** starting the date the order ships.* *If any invoiced amount is not received by the due date, then without limiting Rogue's rights or remedies, (a) out standing amounts will accrue late interest at the rate of 1.5% of the outstanding balance per month, or the maximum permitted by law, whichever is lower, and / or (b) future orders may be conditioned with a reduction or elimination of terms.

* Upon confirmation of this Order, and/or acceptance of the ordered product(s), you hereby agree to these credit terms.

1/24/2022 11:42:27 AM

Page 1 of 1



545 E. 5TH AVE.
COLUMBUS, OH 43201

(614) 358-6190
TEAM@ROGUEFITNESS.COM

OFFICIAL QUOTE

Bill To:
NICK BRANCH
VISALIA FIRE DEPARTMENT
420 N BURKE ST
VISALIA CA 93292

Ship To:
NICK BRANCH
VISALIA FIRE DEPARTMENT
420 N BURKE ST
VISALIA CA 93292

Total Equipment Cost	\$2,700.00
Shipping (UPS Ground)	\$165.00
Tax	\$229.50
Grand Total	\$3,094.50

Quote #: 112193

Weight	Brand	Item Description	SKU	QTY	Price	Total	Details
66 lbs	Concept2	BLACK Concept 2 Model D Rower w/PM5 Monitor	C20003-5	3	\$900.00	\$2,700.00	

* Shipping quotes are only valid for 24 hours

* All previous versions of this form are obsolete

* Please ensure the items and quantities on this quote are correct prior to placing your order

* Custom products require review and approval by the Rogue creative team and may require modifications to be manufactured.

* 100% Due Upon Order unless otherwise Agreed Upon

* All POs are processed with Net 30 terms starting the date the order ships. POs over \$25,000 will require a 50 % deposit to initiate the order. The remaining 50 % of the balance will have Net 30 terms ** starting the date the order ships.* *If any invoiced amount is not received by the due date, then without limiting Rogue's rights or remedies, (a) out standing amounts will accrue late interest at the rate of 1.5% of the outstanding balance per month, or the maximum permitted by law, whichever is lower, and / or (b) future orders may be conditioned with a reduction or elimination of terms.

* Upon confirmation of this Order, and/or acceptance of the ordered product(s), you hereby agree to these credit terms.

1/24/2022 12:14:32 PM

Page 1 of 1



City of Visalia

Visalia City Council
707 W. Acequia
Visalia, CA 93291

Staff Report

File #: 22-0073

Agenda Date: 3/7/2022

Agenda #: 4.

Agenda Item Wording:

Authorize the City Manager to award a sole source three-year contract, with the option to renew for two additional years, for city-wide web hosting and connectivity for fleet Geo Positioning System (GPS) to Cal/Amp for an amount not to exceed \$35,910 per year.

Deadline for Action: None

Submitting Department: Public Works

Contact Name and Phone Number:

Jason Serpa, Public Works Manager, jason.serpa@visalia.city, 713-4533

Wyndi Ferguson, Interim Public Works Director, wyndi.ferguson@visalia.city, 713-4186

Department Recommendation:

Staff recommends that Cal/Amp be awarded the annual web hosting and GPS connectivity contract for a not to exceed amount of \$35,910 per year. This is a sole source recommendation due to the existing GPS units that are currently installed city-wide.

Background Discussion:

Since 2010, Cal/Amp has provided GPS hosting for our city-wide fleet that includes various departments and divisions including Solid Waste, Parks, Buildings, Streets, Wastewater, Airport, and Animal Services. Currently, the City has 127 active GPS units throughout the City fleet.

Due to a mandatory upgrade for 5G network capability and updated pricing, the City can take advantage of lower monthly hosting fees and no replacement costs for obsolete GPS units. Under the old contract, each GPS unit cost the City \$28.00 per month for connectivity. With the new contract pricing, the cost will be lowered to \$19.95 per month per GPS unit. In addition, the GPS units that must be replaced to upgrade to 5G capability would have cost the City \$295 per unit. These 48 obsolete GPS units will be replaced at no additional charge during the first year under the new contract. . Any additional GPS units that are added in future years would cost \$125 per installation.

In order to take advantage of the lower pricing and no charge for the GPS unit upgrade, the City will need to enter into a minimum of a three-year initial contract with the provider as described on the attached price proposal.

The not to exceed amount of \$35,910 is based on 150 GPS units to allow for fleet growth in the later years of the contract. The old and new contract amounts are broken down below:

	Old Contract	New Contract	Savings
Per Unit Monthly	\$28.00	\$19.95	(\$8.05)
Per Unit Annual	\$336.00	\$239.40	(\$96.60)
Total Contract Annual	\$48,720.00	\$35,910.00	(\$12,810.00)
Per Unit Swap for 5G	\$295.00	\$0.00	(\$295.00)
Future Per Unit Install	\$295.00	\$125.00	(\$170.00)

The proposed annual contract for GPS hosting is estimated to be less than the current contract by \$12,810. The GPS hosting service is budgeted in the various departments' operating budgets and will not need a budget amendment.

Fiscal Impact:

The cost for city wide GPS will be paid out of the operating budgets within the various funds for each department/division for which the services are being rendered.

Prior Council Action:

None

Other:

[Click or tap here to enter text.](#)

Alternatives:

Reject staff recommendation and solicit for bids for a City-wide GPS contract. Staff does not recommend this option due to the existing units already installed and being hosted by current provider.

Recommended Motion (and Alternative Motions if expected):

I move to authorize the City Manager to award the City-wide annual web hosting and GPS connectivity sole source contract to Cal/Amp of Irvine, CA for an amount not to exceed \$35,910 per year.

Environmental Assessment Status:

N/A

CEQA Review:

N/A

Attachments:

Cal/Amp Price Proposal

MONTHLY TELEMATICS SERVICE FEES - VEHICLES

150 CALAMP ION VEHICLE TELEMATICS SOLUTION FEES (per Month per Device)
iOn Telematics Web Based Mapping & Reporting
Includes unlimited Web software access.
Service fees are calculated per vehicle per month.
Assumes 30 second update rate, plus on/off, stops/starts, turns, and exception events.

Includes:

- CalAmp Device
- Engine Diagnostics
- Life of contract warranty
- On-site Spare Devices
- Unlimited live online training

per unit

Light Duty: Plug and Play Device	\$ 18.95 each
----------------------------------	---------------

Light Duty or Heavy Duty: Hard-wired Input Capable Device	\$ 19.95 each
---	---------------

Non-Powered Assets/Trailers	\$ 14.95 each
-----------------------------	---------------

Powered Equipment/Trailers	\$ 17.95 each
----------------------------	---------------

Assumes minimum 36 month active service contract.
Minimum Order Quantities may apply for some products.
Volume discounts available.



City of Visalia

Staff Report

Visalia City Council
707 W. Acequia
Visalia, CA 93291

File #: 22-0087

Agenda Date: 3/7/2022

Agenda #: 5.

Agenda Item Wording:

Second Reading and adoption of Ordinance No. 2022-01, approving Conditional Zoning Agreement No. 2021-11, an amendment submitted by CenterPoint Integrated Solutions to Conditional Zoning Agreement No. 2003-01, removing provisions that prohibit the establishment of a “stand-alone” used automobile sales use, and adding development standards for “stand-alone” used automobile sales uses for the Visalia Auto Plaza located within the C-S (Service Commercial) Zone. The Visalia Auto Plaza is located west of North Neeley Street, north of West Camp Drive, and south of the Mill Creek Ditch (APN: 081-020-085).

Deadline for Action: None 3/7/2022

Submitting Department: Community Development

Contact Name and Phone Number:

Cristobal Carrillo, Associate Planner, 713-4443, cristobal.carrillo@visalia.city

[<mailto:cristobal.carrillo@visalia.city>](mailto:cristobal.carrillo@visalia.city)

Paul Bernal, Community Development Director,

713-4025, paul.bernal@visalia.city [<mailto:paul.bernal@visalia.city>](mailto:paul.bernal@visalia.city)

Department Recommendation:

Staff Recommends that the City Council conduct the Second Reading of Ordinance No. 2022-01, approving Conditional Zoning Agreement No. 2021-11, amending Conditional Zoning Agreement No. 2003-01.

Summary:

On February 22, 2022, the City Council held a public hearing for Conditional Zoning Agreement No. 2021-11, a request to amend the design guidelines of the Visalia Auto Plaza master plan (codified via Conditional Zoning Agreement No. 2003-01) to:

1. Remove a prohibition on the establishment of stand-alone used auto dealerships within the Visalia Auto Plaza planning area; and
2. Add a development standard to the Visalia Auto Plaza design guidelines allowing for placement of a stand-alone used automobile dealership within the planning area only if established on a minimum five-acre parcel.

The change to the Conditional Zoning Agreement (CZA) provisions are proposed to facilitate the establishment of CarMax, a used automobile sales and service center within the Visalia Auto Plaza. The Visalia Auto Plaza is an area within the C-S (Service Commercial Zone), located west of North Neeley Street, north of West Camp Drive, and south of the Mill Creek Ditch (APN: 081-020-085).

There was no public opposition at the City Council meeting, and the City Council voted (4-1, Collins No) to approve the proposal and conduct the First Reading of Ordinance No. 2022-01 for Conditional Zoning Agreement No. 2021-11.

Fiscal Impact:

The City of Visalia could potentially see increased sales and district tax revenues from the purchasing and registration of vehicles within the City Limits. Additional economic benefits could be gained from the addition of up to 96 permanent jobs in Tulare County, per an Economic Study prepared in 2020 by the applicant.

Prior Council Action:

- On August 18, 2003 and September 2, 2003 the Visalia City Council approved General Plan Amendment No. 2002-20, Change of Zone No. 2002-18, and Conditional Zoning Agreement No. 2003-01, a request by Mangano Homes/Westland Development to change the land use designation of a site to Commercial Service / C-S (Service Commercial), create design guidelines, and allow up to ten auto dealerships on approximately 72 acres (collectively known as the Visalia Auto Plaza) located west of Plaza Drive between Mill Creek Ditch and Camp Drive (APNs: 081-020-063, 068).
- On August 17, 2020, the Visalia City Council denied Zoning Text Amendment No. 2019-13 and an appeal of Conditional Use Permit No. 2019-42, collectively a request by CarMax to develop a used car sales and service center on a 5-acre parcel within the C-R (Regional Commercial) Zone, and amend the Visalia Municipal Code to allow automobile sales in the C-R Zone. The project site was located on the southwest corner of South Mooney Boulevard and West Visalia Parkway (APN: 126-960-001).
- On February 22, 2022 the Visalia City Council approved Conditional Zoning Agreement No. 2021-11, a request by CenterPoint Integrated Solutions to introduce the first reading of Ordinance No. 2022-01, approving the amendment to Conditional Zoning Agreement No. 2003-11 removing provisions that prohibit the establishment of “stand-alone” used automobile sales uses, and adding development standards for “stand-alone” used automobile sales uses for the Visalia Auto Plaza, located west of North Neeley Street, north of West Camp Drive, and south of the Mill Creek Ditch (APN: 081-020-0071, 072, 078, 084, 085)

Committee/Commission Review and Action:

- On December 13, 2021, the Visalia Planning Commission approved Amendment to Conditional Zoning Agreement No. 2021-11 by a vote of 4 to 0 with Commissioner Gomez absent. No comment in opposition to the proposal was received.

Alternatives: None.

Recommended Motion (and Alternative Motions if expected):

I move to approve the second reading of Ordinance No. 2022-01 for Conditional Zoning Agreement No. 2021-11.

Environmental Assessment Status: No further environmental action is required.

Attachments:

1. Ordinance No. 2022-01 for Conditional Zoning Agreement No. 2021-11
2. Attachment "A" - Visalia Auto Plaza Design Guidelines / Conditional Zoning Agreement No. 2003-01
3. Attachment "B" - Site Plan Exhibits
4. Attachment "C" - Operational Statement

ORDINANCE NO. 2022-01

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF VISALIA
RECOMMENDING APPROVAL OF AMENDMENT TO CONDITIONAL ZONING
AGREEMENT NO. 2021-11: A REQUEST BY CENTERPOINT INTEGRATED
SOLUTIONS TO AMEND CONDITIONAL ZONING AGREEMENT NO. 2003-01,
REMOVING PROVISIONS THAT PROHIBIT THE ESTABLISHMENT OF A “STAND-
ALONE” USED AUTOMOBILE SALES USE, AND ADDING DEVELOPMENT
STANDARDS FOR “STAND-ALONE” USED AUTOMOBILE SALES USES FOR THE
VISALIA AUTO PLAZA LOCATED WITHIN THE C-S (SERVICE COMMERCIAL) ZONE.
THE VISALIA AUTO PLAZA IS LOCATED WEST OF NORTH NEELEY STREET,
NORTH OF WEST CAMP DRIVE, AND SOUTH OF THE MILL CREEK DITCH
(APN: 081-020-085)

WHEREAS, the amendment to Conditional Zoning Agreement No. 2021-11 is a request by CenterPoint Integrated Solutions to amend Conditional Zoning Agreement No. 2003-01, removing provisions that prohibit the establishment of a “stand-alone” used automobile sales use, and adding development standards for “stand-alone” used automobile sales uses for the Visalia Auto Plaza located within the C-S (Service Commercial) Zone. The Visalia Auto Plaza is located west of North Neeley Street, north of West Camp Drive, and south of the Mill Creek Ditch (APN: 081-020-085); and

WHEREAS, the Planning Commission of the City of Visalia, after duly published notice, held a public hearing before said Commission on December 13, 2021, considered the amendment to Conditional Zoning Agreement No. 2021-11 in accordance with Section 17.60.030 and Section 1760.050 of the Zoning Ordinance of the City of Visalia and on the evidence contained in the staff report and testimony presented at the public hearing; and

WHEREAS, the Planning Commission adopted Resolution No. 2021-60, recommending that the City Council approve Amendment to Conditional Zoning Agreement No. 2021-11, adopting the requested amendments removing provisions that prohibit the establishment of a “stand-alone” used automobile sales use, and adding development standards for “stand-alone” used automobile sales uses for the Visalia Auto Plaza located within the C-S (Service Commercial) Zone; and,

WHEREAS, the City Council of the City of Visalia, after duly published notice, held a public hearing on January 18, 2022, and considered the amendment to Conditional Zoning Agreement No. 2021-11 in accordance with Section 17.60.040 and Section 1760.050 of the Zoning Ordinance of the City of Visalia and on the evidence contained in the staff report and testimony presented at the public hearing; and,

WHEREAS, Categorical Exemption No. 2021-52 was prepared for the project consistent with the California Environmental Quality Act, Section No. 15305, which is appropriate as the project as the project will not produce significant changes in land use or density; and

WHEREAS, the City Council of the City of Visalia finds as follows:

1. That the Amendment to Conditional Zoning Agreement No. 2021-11 is compatible with existing and planned land uses on the site and surrounding areas as the change will continue to facilitate the sale of automobiles, on sites compatible in size to adjacent parcels.
2. That the Amendment to Conditional Zoning Agreement No. 2021-11 will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
3. That Categorical Exemption No. 2021-52 was prepared for the project consistent with the California Environmental Quality Act, Section No. 15305, which is appropriate as the project will not produce significant changes in land use or density.

NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF VISALIA:

That Amendment to Conditional Zoning Agreement No. 2021-11 is approved as contained in Exhibit "A" of this Ordinance.

**AMENDED AND RESTATED CONDITIONAL ZONING AGREEMENT
NO. 2003-01**

THIS AMENDED AND RESTATED AGREEMENT is made this ____ day of ____ 2021, by BP Pearla Properties LP, hereinafter called the “Owner” and the CITY OF VISALIA, a municipal corporation and charter law city of the State of California, hereinafter called, “City.”

WITNESSETH

WHEREAS, on August 26, 2003, American Properties Holdings, LLC and the City entered into Conditional Zoning Agreement No. 2003-01 (“CZA No. 2003-01”) which required certain conditions to the rezoning of certain property situated in the City of Visalia, hereinafter referred to as “the Property” which is more particularly described in Exhibit I of this Agreement; and

WHEREAS, prior to CZA No. 2003-01, the Property was zoned as specified in Item (a) of Exhibit I; and

WHEREAS, as part of the approval process and adoption of CZA No. 2003-01, the Property was rezoned to the classifications further specified in Item (a) of Exhibit I; and

WHEREAS, during the hearings held before the City Council of the City of Visalia, State of California, and after having considered the matter presented, certain conditions were imposed and included in CZA No. 2003-01 so as not to create any problems inimical to the health safety and the general welfare of the City of Visalia and its residents; and

WHEREAS, CZA No. 2003-01 was entered into between American Properties Holdings, LLC, a California limited liability company, Land Securities Investors, LTD., a Colorado limited partnership, and the City of Visalia, and recorded with the Tulare County Recorder on October 3, 2003, as Document No. 2003-0096292; and

WHEREAS, after to the adoption of CZA No. 2003-01, title to the Property was transferred to BP Pearla Properties LP; and

WHEREAS, BP Pearla Properties LP, through its designated agent CenterPoint Integrated Solutions and CarMax, wishes to amend the agreement to allow for the development of a CarMax used automobile sales and service facility on the Property and has submitted an application to revise certain conditions of Amended CZA No. 2003-01, limited to removal of the prohibition on the establishment of standalone used auto sales dealerships, subject to a five acre minimum lot size limitation, as set forth in Exhibit I; and

WHEREAS, it is deemed necessary to execute this “Amended and Restated Conditional Zoning Agreement No. 2003-01” in order to properly reflect current ownership, conditions and remaining obligations for completing development of the Property.

NOW, THEREFORE, IT IS MUTUALLY UNDERSTOOD AND AGREED that inasmuch as the rezoning specified in Item (b) of Exhibit I has been granted subject to the conditions specified in the following paragraphs:

1. That this “Amended and Restated Conditional Zoning Agreement No. 2003-01 shall replace and supersede those portions of CZA No. 2003-01 dealing with prohibition of standalone used

automobile sales, and that all other portions of the prior Amendment are not intended to be altered or modified by this Amendment.

2. That Exhibit I, as completed and attached hereto, is incorporated into and made a part of this Agreement with the same force and effect as if fully set forth herein.

3. That First Parties shall comply with the additional conditions, if any, specified in Item (e) of Exhibit I of this Agreement.

4. In the event the Owner, or any successor in interest to Owner, or any person in lawful possession of the Property described in Item (d) of Exhibit I violates or fails to perform any of the conditions of this Agreement within, thirty (30) days after notice thereof has been provided in accordance with Section 6, the City Council of the City of Visalia may instruct the City Attorney of the City of Visalia to institute legal proceedings to enforce the provisions of this Agreement. Any failure by the Owner to perform or comply with any of the conditions under Item (e) of this agreement within thirty (3) days after written notification from City to do so, shall constitute a default in this agreement by the Owner. In addition to any other remedy it may have at law or equity, City may, but is not obligated to, perform any such conditions or requirements set forth in Item (e) of this agreement either through its own employees or through parties with whom it may contract, Owner shall reimburse City said costs and expenses within thirty (3) days after a written invoice from the City.

5. The prevailing party in any legal action arising from the failure of either party to perform the duties and obligations required by and contained in this Agreement shall be entitled to reasonable attorney's fees, expert witness fees and costs of suit.

6. Notice of violation of provisions of this Agreement shall be sent to the Owner at the addresses specified in Item (c) of Exhibit I. Any subsequent title holder, any lien holder, or party in possession of the Property shall also receive notice of such violation if they are reflected on the tax roll.

7. Each and every one of the provisions of this Agreement herein contained shall run with the land and bind and inure to the benefit of the successor in interest of each and every party hereto, in the same manner as if they had herein been expressly named.

8. Zoning of the Property as indicated in Item (b) of Exhibit I will be consummated when this "Amended and Restated Conditional Zoning Agreement No. 2003-01" is recorded in the office of the Tulare County Recorder.

9. This Agreement is the product of negotiation and compromise on the part of each party and the parties agree, notwithstanding Civil Code § 1654, that in the event of uncertainty the language will not be construed against the party causing the uncertainty to exist.

10. This Agreement may be amended by mutual consent if in writing and, in the case of City, authorized by its City Council.

IN WITNESS WHEREOF, the parties hereto have executed this Agreement the day and year first above written.

“Owner”

BP PEARLA PROPERTIES LP

BY: _____

“City”

CITY OF VISALIA, A political subdivision
of the State of California

ATTEST: _____

Chief Deputy City Clerk

BY: _____

City Manager

EXHIBIT I

AMENDED AND RESTATED CONDITIONAL ZONING AGREEMENT NO. 2003-01

(a) The property described in Exhibit “A,” was originally zoned Agriculture (A) prior to the entering of CZA No. 2003-01.

(b) Upon the entering into CZA No. 2003-01, the classification changed from its present zoning to Service Commercial (C-S).

(c) Notice to Owner shall be addressed to:

BP Pearla Properties LP
PO Box 1530
Turlock, CA 95381-1530

(d) “Property” as used in this Agreement, includes:

APN Nos. 081-020-071, 072, 078, 084, 085, located within the Visalia Auto Plaza west of Neeley Street, north of Camp Drive, and south of the Mill Creek Ditch, in the City of Visalia, Tulare County, State of California.

(e) The additional conditions with which the Owner shall comply, pursuant to Paragraph No. 3, of this Agreement are as follows:

1) That the Design Guidelines attached as Exhibit II apply to the subject parcels and any future parcels.

2) That Page 4, Site Planning Concepts Item “B” of the Design Guidelines attached as Exhibit II shall be amended as follows:

~~B. Proposed uses; Uses within the project and restricted to new car dealerships with ancillary used car sales and normal service components. No stand alone used car facilities will be allowed in the project.~~ *That uses within the Visalia Auto Plaza are restricted to new automobile dealerships, with ancillary used car sales and normal service components, and standalone used automobile dealerships on parcels of no less than five-acres in size, with ancillary normal service components.*

EXHIBIT II

**CONDITIONAL ZONING AGREEMENT NO. 2003-01
Design Guidelines**

VISALIA AUTO PLAZA

Architectural Design Guidelines

August 19, 2003

Prepared By:

QK4

Architecture.Engineering.Construction.

TABLE OF CONTENTS

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Site Planning Concepts	4
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Parking Requirements	5
Design Review / Submittal Process / General Architecture	6
Landscaping	11
Signage	12
Lighting	15

Visalia Auto Plaza

Auto Mall

INTRODUCTION

Visalia Auto Plaza (VAP) is a 70 acre, state of the art, regional automotive shopping and service experience for customers in the greater Visalia / Tulare / Hanford area. Modeled after retail concepts of the regional shopping mall, the VAP provides shoppers a unique destination where multiple vehicle dealerships are consolidated on one property of facilitate their purchase and service needs. Variety and selection are the two primary goals of any shopping experience and the VAP through the introduction of separate vehicles brands on properties ranging from approximately 4 to 13.5 acres.

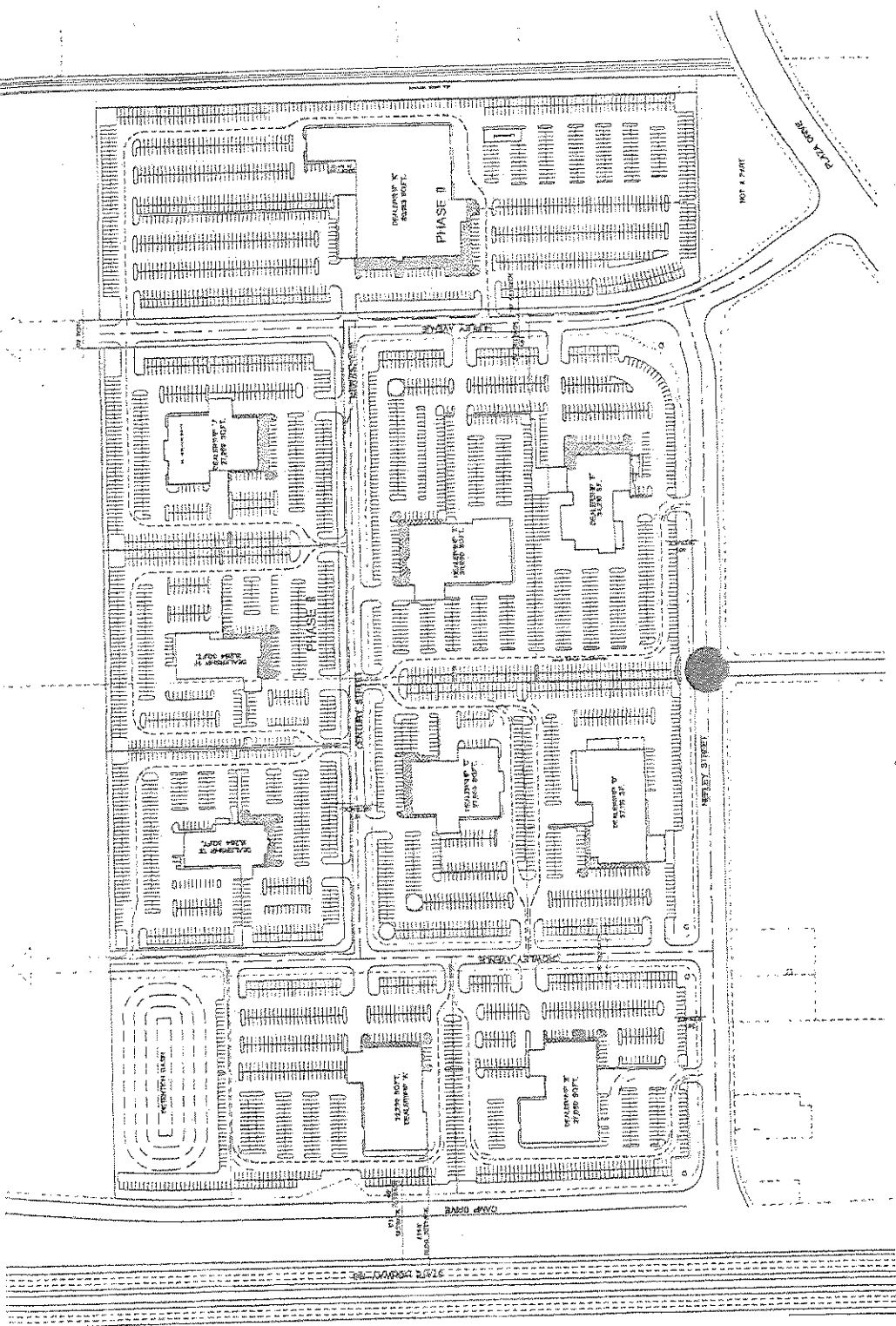
The following architectural design standards have been created as guidelines for the development of the auto plaza and to create continuity between each facet of the project as it is designed and constructed.

SITE MASTER PLAN

The overall Site Master Plan for the Visalia Auto Plaza is shown with all of the proposed phases, is reduced and shown on the next page, Exhibit 1.

SITE IMPROVEMENT

Site improvements will be constructed as each individual dealership develops. These improvements will include construction of ½ streets, plus a 12 foot wide travel lane as well as frontage improvements such as curb and gutter, landscaping and street lighting along the project side only.



CONCEPT PLAN
FIGURE 6



- * INTER: INTERSECTIONS AND ARE INDICATED BY A CIRCLE WITH A DOT
- * PHASE I DEVELOPMENT ARE HATCHED AREAS
- * PHASE II DEVELOPMENT ARE HATCHED AREAS
- * PHASE III DEVELOPMENT ARE HATCHED AREAS
- * PHASE IV DEVELOPMENT ARE HATCHED AREAS
- * PHASE V DEVELOPMENT ARE HATCHED AREAS
- * PHASE VI DEVELOPMENT ARE HATCHED AREAS
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- * PHASE XI DEVELOPMENT ARE HATCHED AREAS
- * PHASE XII DEVELOPMENT ARE HATCHED AREAS
- * PHASE XIII DEVELOPMENT ARE HATCHED AREAS
- * PHASE XIV DEVELOPMENT ARE HATCHED AREAS
- * PHASE XV DEVELOPMENT ARE HATCHED AREAS
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- * PHASE XXVI DEVELOPMENT ARE HATCHED AREAS
- * PHASE XXVII DEVELOPMENT ARE HATCHED AREAS
- * PHASE XXVIII DEVELOPMENT ARE HATCHED AREAS
- * PHASE XXIX DEVELOPMENT ARE HATCHED AREAS
- * PHASE XXX DEVELOPMENT ARE HATCHED AREAS

SOURCE: QK1 ARCHITECTURE

Visalia Auto Plaza

Auto Mall

SITE PLANNING CONCEPTS

- A. Building location and open spaces; Visalia Auto Plaza auto mall is planned on a parcel of land that is within the gateway to the City of Visalia along State Highway 198. Careful consideration was given to the appearance and site lines from the highway. The building orientation and land uses that abut the highway were purposely laid out to enhance this scenic corridor. Therefore the intent was not to back the project up to the highway, but to open it up and allow site lines with views into the center. To enhance the concept, peripheral building elements (landscape, hardscape, screen walls, sign bases, etc.) will have unifying architectural styles. Buildings will be built on stand-alone pads.
- B. Proposed uses; Uses within the project and restricted to new car dealerships with ancillary used car sales and normal service components. No stand alone used car facilities will be allowed in the project.
- C. Access; Access to the Auto Plaza will be from Plaza Drive. Ingress and egress for individual dealerships will be from Neeley, Hurley, Century, and Crowley Streets. No access to individual parcels will be permitted from Camp Drive.
- D. Phasing of Construction; The project will be constructed in two phases. The developer will construct all improvements to the public streets, underground utilities, common area landscaping, and signage at each phase. Construction of improvements for each parcel would be the responsibility of individual parcel owner. Each individual parcel will provide the required parking for their building. Site drainage will designed so that each phase will work independently of the other phases.

REQUIRED SETBACKS

Building and landscape setbacks are required and will be established through the Architectural Design Guidelines. Setbacks are to be measured from the edge of the property boundary unless otherwise indicated.

A. Building Setbacks:

- | | |
|-------------------------|---|
| • Hurley, Camp & Neeley | 30' Minimum |
| • Century, & Crowley | 15' Minimum |
| • Mill Creek | 5' Minimum from edge of Conservation Easement |
| • Side Yard | 5' Minimum |
| • Rear Yard | 5' Minimum |

Visalia Auto Plaza

Auto Mall

B. Landscaping Setback:

- Camp, Hurley & Neeley 30' Minimum
- Century, Crowley 15' Minimum
- Mill Creek Ditch 5' Minimum from edge of Conservation Easement
- Side Yard 5' Minimum
- Rear Yard 5' Minimum

C. Building Coverage:

- Each site shall have a maximum building coverage of fifty (50) percent of the net site area.

D. Building Height:

- Showroom building height shall be to a maximum of thirty (30) feet. The height shall be measured from the finished surface grade level to the highest point of the roof.

PARKING REQUIREMENTS

A. Customer and employee parking shall be incorporated on the site plans and shall clearly be identified by on-site signage.

B. Minimum parking requirements are as follows:

- Automobile dealerships; One parking space for each two employees during the time of maximum employment, plus one parking space for each two thousand (2,000) square feet of lot and building area used for the display or storage of automobiles.
- No parking of vehicles for sale will be allowed in the public right of way or landscape setbacks (with the exception of raised dealer display pads)

C. Loading and Unloading Facilities:

- Each Dealership site shall provide a loading and unloading area which shall be a minimum of twelve (12) feet by 105 feet. This special area shall be designated on the site plan and shall be in conjunction with the inventory storage area. The design of the loading and unloading area shall provide for the following:
- Incorporate ease of ingress and egress for safety and efficiency.
- The loading and unloading facilities shall not adversely interfere with on-site circulation.

Visalia Auto Plaza

Auto Mall

- The loading and unloading facilities shall be designed to accommodate vehicle carrier truck maneuvers on site, without backing from or into a public street.

DESIGN REVIEW / SUBMITTAL PROCESS / GENERAL ARCHITECTURE

A design review and submittal process has been established to ensure that all elements of the development within the Visalia Auto Plaza meet the requirements set forth in these design guidelines. All plans and specifications will be submitted through City of Visalia's Site Plan Review process.

- A. No building, exterior signs or structures shall be erected or exterior alterations or additions made on any site unless such improvements are in conformance with the plans and specifications approved by the Architectural Review Committee. The approval shall encompass landscaping and architecture as specified by these standards.
- B. The Architectural Review Committee shall be established prior to development of the auto dealership and will consist of the following members:
 - Two (2) Developer representatives – Auto Plaza Associates, LLC
 - Two (2) Dealership representatives –
 - One (1) Architectural representative –
 - One (1) City representative as appointed by the City Planner -
- C. Processing through the Auto Plaza Architectural Review Committee will be in addition to the reviews and submittals required by the City of Visalia. Approval or disapproval shall be made in writing within 30 days of application.
- D. Submittals to the Architectural Review Committee shall include:
 - A Site Plan showing the location of all of the buildings, landscaped areas, on-site parking areas, vehicular and pedestrian access ways, accessory structures and signage.
 - Drawings shall be submitted showing elevations of all buildings from all ground level views together with landscaping, accessory structures, building colors and rim materials.
 - A color board of all the surface materials of the buildings and accessory structures.
 - All plans shall be prepared by licensed, registered personnel appropriate to the designs being submitted.

Visalia Auto Plaza

Auto Mall

- Additional supporting material shall be submitted which adequately demonstrate, to the satisfaction of the Architectural Review Committee, that the provisions of these guidelines are met.
- Landscaping plans shall be submitted which show plant selection and usage, conceptual grading, drainage and irrigation improvements.

E. Architectural Theme:

- The Visalia Auto Plaza intends to encourage a Modern/Contemporary architectural theme. The intent of these standards is not to limit innovative design approaches, nor is it to have all dealerships look exactly alike. Manufacturer image programs are acceptable.
- All buildings should be simple in form and should emphasize geometric shapes and horizontal line qualities. The use of curved surfaces is encouraged.

F. Colors:

- The color scheme shall emphasize earth tones and warm gray shades. The buildings' body color that is selected shall be used through each individual dealership and shall work to integrate all elements of the individual dealerships.

G. Building Materials Acceptable:

- Concrete masonry units
- Stucco
- Exposed timbers
- Precast concrete tilt-up panels.
- Ornamental iron
- New brick of rust color
- Flush surface metal panels

H. Buildings Materials Unacceptable:

- Exposed wood siding
- Corrugated metal siding
- Unpainted metal trim or flashing
- Shake or shingle roof

I. Building Orientation and Views:

Visalia Auto Plaza

Auto Mall

- The dealership showrooms and exterior covered areas shall be oriented toward Hurley, Century, and Crowley Streets. Public entrances should be identified by color or form.
- Building and exterior display areas should be sited to complement existing topography, site configuration and adjacent uses. Onsite grading should be kept at a minimum.
- Service bays and repair areas are to be enclosed and located to the rear of the site area. Loading areas, storage areas and trash collection areas should be screened from view from public streets, pedestrian walks or common customer parking areas. The screen wall should be a minimum of six (6) feet in height and shall be masonry concrete or stucco construction consistent with the buildings materials used on the main building. Site fencing, where such fencing is not visible from public streets, may be chain link to maximum height of seven (7) feet with an eighteen (18) inch spiral razor barbed wire cap. No fencing shall be permitted in the front yard setback.
- Roof mounted mechanical equipment, ductwork, vents and access ladders shall be screened from view from the public streets, State Highway 198, pedestrian walks and common parking areas. The equipment shall be screened by the building from itself using parapets, roofs, etc. Individual equipment screens are not encouraged unless they are determined to be consistent with the general building design and materials by the Architectural Review Committee.
- Views from Highway 198; Six (6) foot screen walls will be used to minimize views of the service bays from the highway.
- Large wall masses are to be broken-up through the use of a combination of varying colors and or materials.

J. Vehicle Display Pads:

- Each parcel may have, except on the Camp Drive and Neeley Street frontages, two (2) raised vehicle display pads for each manufacturer line to a maximum of six (6) per parcel. All display pad shall be constructed consistent with the design in Exhibit 2 and may be located in the required landscape area a minimum of five (5) from property line and shall not create a site distance impact to vehicle traffic when exiting. Suggested distance from vehicle access drives is twenty-five (25) feet.
- Vehicle Display pads may also be used to enhance the Visalia Auto Plaza Center Identification Sign Area as shown in Exhibit 3 if the property owner/dealership association includes a mechanism for the sharing of these pads by all the dealerships on a rotating basis. Otherwise, raised pads are not allowed within 50 feet of the center identification signs.

Exhibit "2"

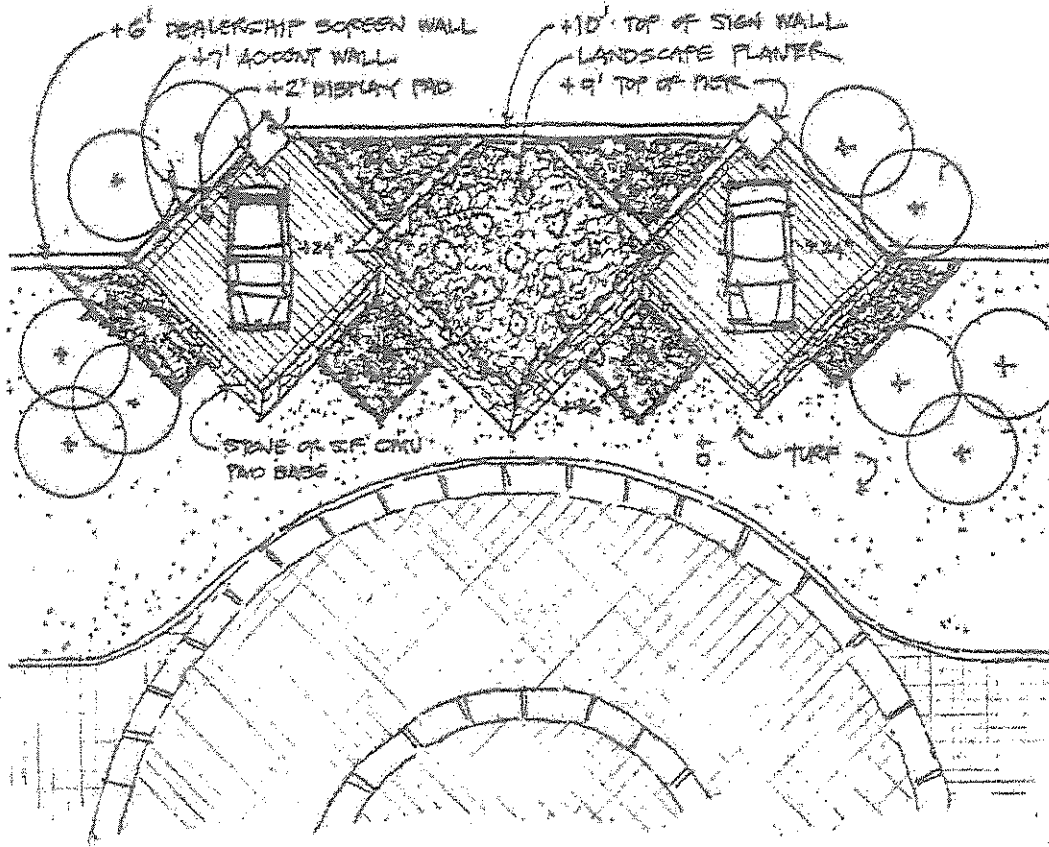
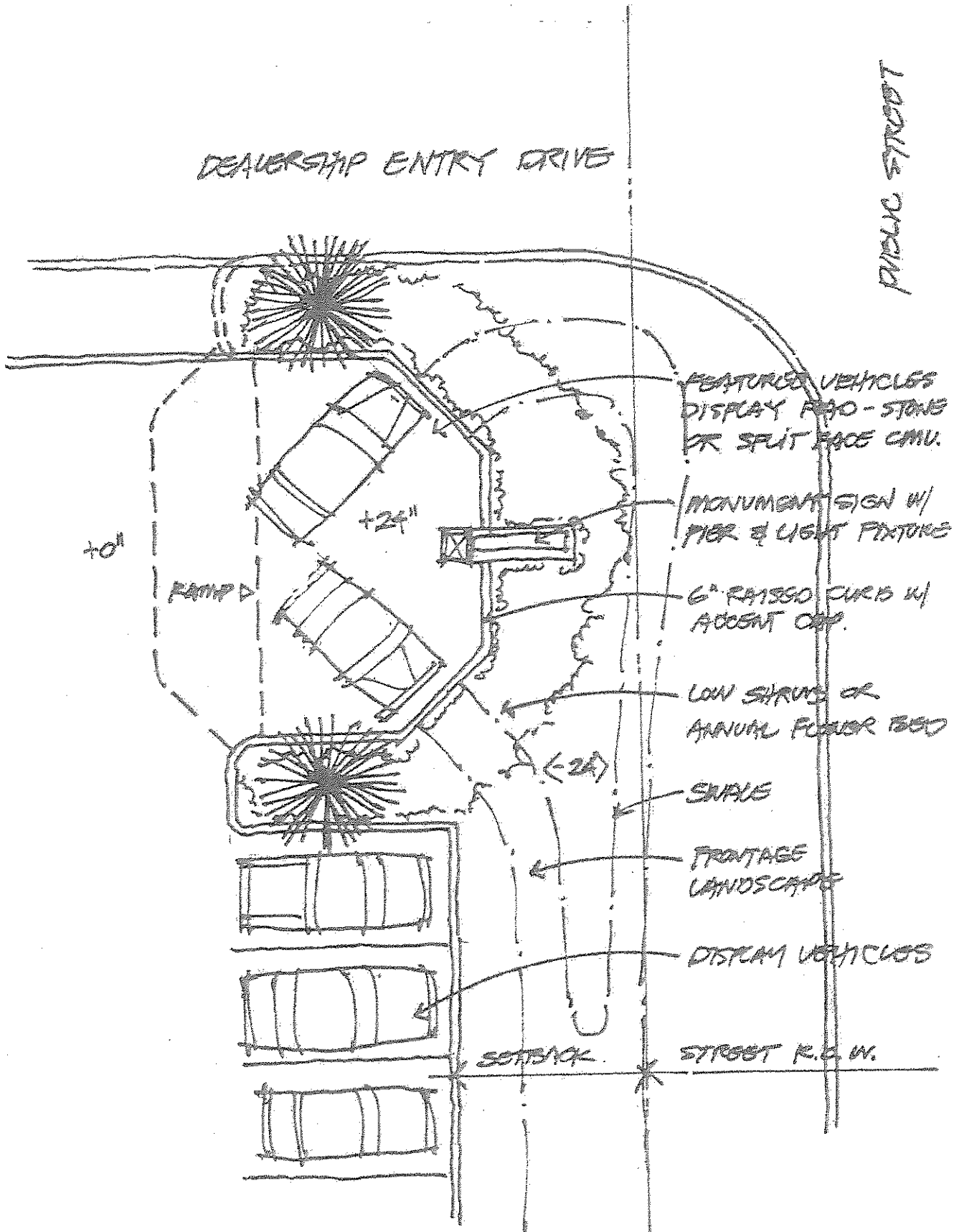


Exhibit "3"



Visalia Auto Plaza

Auto Mall

LANDSCAPING

The primary objective of the landscape guidelines is to promote a visual separation between the dealers, parking lots and adjacent properties. The goal is to promote an environment within the auto mall that is pleasing, inviting, and unified.

A. Landscaping:

- All landscaping will be in accordance with section 17.30.130 of the City of Visalia's Zoning Ordinance, with a provision for clustering of trees.
- Every site on which a building is placed shall be landscaped and maintained thereafter in a well kept condition. A landscaping plan shall be submitted to the Architectural Review Committee. Such landscaping plan shall be prepared by a landscape architect licensed in the State of California.
- Interior landscaping shall be utilized to break up large paved areas and soften building lines.
- The thirty (30) foot front landscaping shall be gently mounded to maximum height of eighteen (18) inches utilizing a turf or ground cover treatment. Trees and shrubs shall be used at common property lines to define site separations.
- The Mill Creek Conservation Easement will be landscaped with riparian plantings.

B. Soil Preparation:

- To ensure proper root growth, soil tests shall be performed and soil additives used to provide adequate nutrients, rooting environmental and soil Ph.

C. Irrigation:

- All landscaped planting areas shall be fully irrigated with complete coverage. All systems/circuits shall be underground. Detailed landscaping and irrigation plans shall be submitted to the City of Visalia Planning Department for review and approval prior to issuance of building permits.
- All irrigation systems shall be automatically controlled with electromechanical or solid state controllers able to have variable length timing circuits.
- Water conservation systems are recommended.
- All valves (remote control valves, quick coupler valves, shut-off valves, etc.) shall be installed in valve boxes which are flush to finished landscaped grade.

Visalia Auto Plaza

Auto Mall

D. Fencing:

- On-site fencing may include a chain fence to a maximum total height of seven (7) feet with an optional eighteen (18) inch spiral razor ribbon barbed wire cap, where such fencing is not visible from the public right of way, public walkways and where it does not extend into the thirty (30) foot front yard setback. Masonry walls of the same height may be substituted at owner's option, or where required by the City of Visalia. This requirement is designed to ensure security for individual properties.

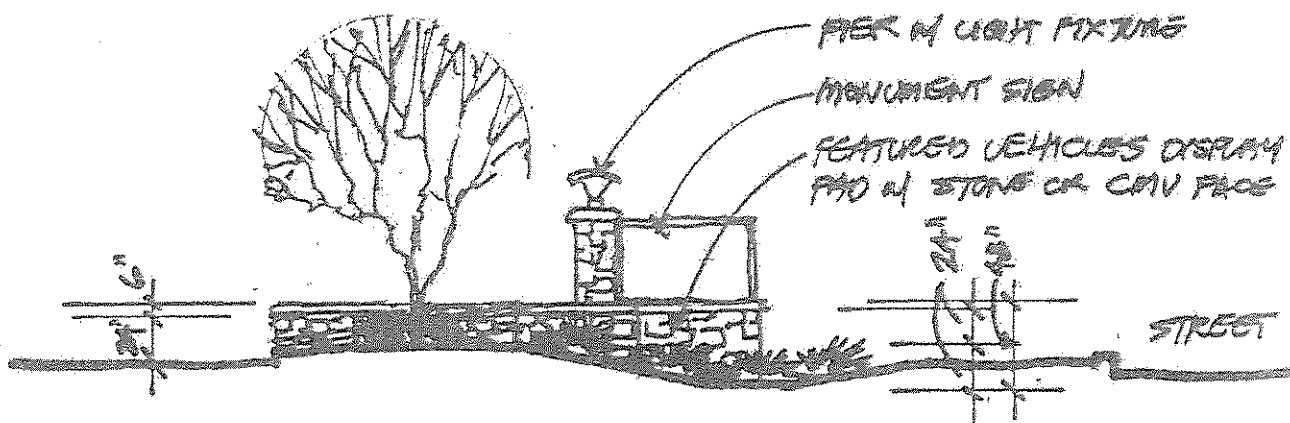
SIGNAGE**A. General Requirements**

- Each dealership shall be allowed only those signs that are necessary for identification of the franchise, the premises, the department uses on site an directional signing. The design size, location, color and materials of construction shall be submitted to the Architectural Review Committee for approval prior to construction. All dealerships shall comply with the City of Visalia Sign Ordinance except as provided herein. Billboards, streamers, temporary signs, revolving signs, flashing signs and moving signs are specifically prohibited.
- No signs visible from outside the property may be placed, parked, attached to or displayed from the site except as provided herein. No signs projecting from the roof line of any building or painted on the sides of buildings or roof without approval of the Architectural Review Committee.
- No temporary signs or advertising materials such as balloons, streamers, flags, and inflatable advertising shall be permitted.
- Center identification signs shall only be allowed at the southwest corner of Hurley and Neeley Streets and the T-intersection of Crowley and Neeley as shown in Exhibit 3.

B. Free Standing Monument Signs

- Each dealership shall be allowed one double faced monument sign for each auto line to a maximum of two per parcel consistent with the sign design in Exhibit 4. The sign shall be located on the primary street frontage. Such sign shall be ground-mounted containing the dealership name and manufacture name of the new automobiles sold on the site. The maximum height of the sign shall not exceed ten (10) feet. The total illuminated sign area shall not exceed thirty-five (35) square feet of sign copy per face. The total aggregate surface area of the sign faces and the structure shall not exceed one hundred forty (140) square feet.

Exhibit "4"



Visalia Auto Plaza

Auto Mall

- Freestanding monument signs shall be setback a minimum of five (5) feet from front property lines and a minimum of twenty (20) feet from any interior side property line.
- Freestanding signs are not allowed on the Camp Drive frontage.

C. Building Signs

- Each dealership shall be allowed building signage identifying the dealership name and manufacture name of new automobiles sold on the site. The sign(s) shall be located on the top showroom fascia facing the primary street or on a flat wall surface. The signs shall be preformed, individual illuminated letters with Plexiglas faces in deep channel cans. The maximum height of the letters shall be thirty-six (36) inches. Logo signs shall not exceed ten (10) square feet in area.
- Building signs along Camp Drive shall be limited to secondary signage only.
- Primary Sign - Two square feet of sign area is permitted for each foot of linear occupancy frontage to a maximum of one hundred fifty (150) square feet. Dealerships may choose which exterior side of the building will be used for the purpose of calculating the permitted sign area.
- Secondary Sign - Additional signs of a maximum of twenty-five (25) percent of the sign area as calculated for the primary sign shall be allowed for each remaining exterior wall provided that the sign area for any given wall shall not exceed two square feet per linear foot of the wall length.
- Variances to building sign area may be approved by the Planning Commission in accordance with Section 17.48.110 of the City of Visalia Zoning Ordinance. Approval of a variance shall be based on providing adequate signage for all manufacturer lines at a single dealership.

D. Department Identification Signs

- One wall sign is allowed for each department (e.g., use card, service, parts, body shop, etc). The maximum height of such signs shall be eight (8) inches with a maximum total area of three square feet per sign.

E. Directional signs

- Dealership directional signs shall be limited to "Enter", "Exit", "Service Entrance", "Customer Parking", and "Employee Parking". Sign height shall not exceed four (4) feet with a maximum sign area of four (4) square feet.

Visalia Auto Plaza

Auto Mall

LIGHTING

Illumination of the auto mall is important for the safety and security of the pedestrian, and motorist. The quality of the lighting needs to be maintained throughout the center and reinforced through the consistent use height, spacing, color and type of fixture used with in the area.

- Although lighting plans for the auto mall have not yet been developed, a general concept for project lighting to be employed in the phase I development has been established. The plan for night lighting for phase I development would provide for sufficiently bright lighting for security and safety purposes, but would avoid offensive glare and direct illumination of off-site locations. This would be accomplished by specifying the use of metal halide lighting in the parking lots, display areas, and internal circulation areas, by recessing light source completely within their fixture, and through the use of cut-off shields on light standards to block direct illumination beyond the parcel boundaries.
- Search Lights (temporary or permanent) are prohibited.



27

RECEIVED

2003-0096292

NO FEE REQUIRED PURSUANT
TO GOVT. CODE SECTION 27383
RECORDING REQUESTED BY
AND MAIL RESPONSE TO:

DEC 23 2003

COMM. DEVELOP.
CITY OF VISALIA

Recorded
Official Records
County Of
Tulare
GREGORY B. HARDCASTLE
Recorder

REC FEE .00

11:06AM 03-Oct-2003

BT
Page 1 of 23

City of Visalia
Planning Division
315 East Acequia
Visalia, CA 93291

CONDITIONAL ZONING AGREEMENT NO. 2003-01

THIS AGREEMENT made this 26th day of August 2003, by American Properties Holdings, LLC hereinafter called "Owner" and the CITY OF VISALIA, a municipal corporation and charter law city of the State of California, hereinafter called, "City."

WITNESSETH

WHEREAS, the Owner is the owner of real property, herein called the "Property" situated in the City of Visalia, which Property is described in Item (d) of Exhibit I of this Agreement; and

WHEREAS, the Property is now zoned as specified in Item (a) of Exhibit I; and

WHEREAS, the Owner has applied for a rezoning of the Property pursuant to which application the Property is being rezoned from its present classification to the classification or classifications specified in Item (b) of Exhibit I; and

WHEREAS, hearings have been held upon said application before the City Council of the City of Visalia, State of California, and after having considered the matter presented, it has been determined that certain conditions to the rezoning of said real property must be imposed so as not to endanger the health, safety, and the general welfare of the City of Visalia and its residents.

NOW, THEREFORE, IT IS MUTUALLY UNDERSTOOD AND AGREED that inasmuch as the rezoning specified in Item (b) of Exhibit I is being granted, the rezoning shall be subject to the conditions specified in the following paragraphs:

1. That Exhibit I, as completed and attached hereto, is incorporated into and made a part of this Agreement with the same force and effect as if fully set forth herein.
2. That the Owner shall comply with the additional conditions specified in Item (e) of Exhibit I of this Agreement.
3. In the event the Owner, and successor in interest of the Owner, or any person in possession of the property described in Item (d) of Exhibit I violates or fails to perform any of the conditions of this Agreement within thirty (30) days after notice thereof as provided in Paragraph 4, the City may institute legal proceedings to enforce the provisions of this Agreement. Any failure by the Owner to perform or comply with any of the conditions under Item (e) of this agreement within thirty (30) days after written notification from City to do so, shall constitute a default in this agreement by the Owner. In addition to any other remedy it may have at law or equity, City may, but is not obligated to, perform any such conditions or requirements set forth in Item (e) of this agreement either through its own employees or through parties with whom it may contract,

Owner shall reimburse City said costs and expenses within thirty (30) days after a written invoice from the City.

4. The prevailing party in any legal action arising from the failure of either party to perform the duties and obligations required by and contained in this Agreement shall be entitled to reasonable attorney's fees, expert witness fees and costs of suit.
5. Notice of violation of provisions of this Agreement shall be sent to the Owner at the address specified in Item (c) of Exhibit I. Any subsequent title holder, any lien holder, or party in possession of the Property shall also receive notice if they are reflected on the tax roll.
6. Each and every one of the provisions of this Agreement herein contained shall run with the land and shall bind and inure to the benefit of the successors in interest, heirs and assigns, in the same manner as if they had herein been expressly named.
7. Zoning of the Property as indicated in Item (b) of Exhibit I shall not be consummated until such time as the Agreement has been recorded in the office of the Tulare County Recorder.
8. This Agreement is the product of negotiation and compromise on the part of each party and the parties agree, notwithstanding Civil Code § 1654, that in the event of uncertainty the language will not be construed against the party causing the uncertainty to exist.
9. This Agreement may be amended by mutual consent if in writing and, in the case of City, authorized by its City Council.

IN WITNESS WHEREOF, the parties hereto have executed this Agreement the day and year first above written.

"Owner"

American Properties Holdings, LLC,
a California limited liability company

Land Securities Investors, LTD.,
a Colorado limited partnership

By: Sunset Management Services, Inc.
a Colorado Corporation/ General Partner
By: Alan R. Fishman

Alan R. Fishman, President

Velma Dyck
Velma Dyck

"City"

City of Visalia,
a municipal corporation

By: [Signature]
City Manager

ATTEST: [Signature]
Clerk of the City Council

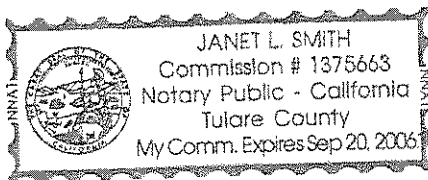
CALIFORNIA ALL-PURPOSE ACKNOWLEDGMENT

State of California

County of Tulare } ss.

On Oct. 2, 2003 before me, Janet L. Smith Notary Public
Date Name and Title of Officer (e.g., "Jane Doe, Notary Public")
 personally appeared Steven M. Salomon
Name(s) of Signer(s)

☒ personally known to me
☐ proved to me on the basis of satisfactory evidence



to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

WITNESS my hand and official seal.

Janet L. Smith
Signature of Notary Public

OPTIONAL

Though the information below is not required by law, it may prove valuable to persons relying on the document and could prevent fraudulent removal and reattachment of this form to another document.

Description of Attached Document

Title or Type of Document: _____

Document Date: _____ Number of Pages: _____

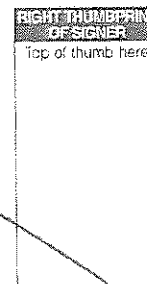
Signer(s) Other Than Named Above: _____

Capacity(ies) Claimed by Signer

Signer's Name: _____

- ☐ Individual
☐ Corporate Officer — Title(s): _____
☐ Partner — ☐ Limited ☐ General
☐ Attorney-in-Fact
☐ Trustee
☐ Guardian or Conservator
☐ Other: _____

Signer Is Representing: _____



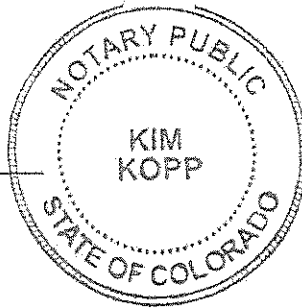
STATE OF COLORADO
COUNTY OF JEFFERSON }

On, Aug. 26, 2003, before me, Kim Kopp, Notary Public personally appeared Alan R. Fishman, personally known to me, or, proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

WITNESS my hand and official seal.

Kim Kopp

My Commission Expires January 9, 2006



RE: Conditional Zoning Agreement No. 2003-01

STATE OF CALIFORNIA

}

ss.

COUNTY OF TULARE

On, October 1, 2003, before me, R. Hensley, Notary Public personally appeared Velma Dyck, personally known to me - OR - proved to me on the basis of satisfactory evidence to be the person~~(s)~~ whose name~~(s)~~ is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity~~(ies)~~, and that by his/her/their signature~~(s)~~ on the instrument the person~~(s)~~, or the entity upon behalf of which the person~~(s)~~ acted, executed the instrument.

WITNESS my hand and official seal.

R. Hensley



Re: Conditional Zoning Agreement No. 2003-01

EXHIBIT I
CONDITIONAL ZONING AGREEMENT NO. 2003-01

- (a) The Property prior to the rezone contemplated in this agreement is now zoned:

Agriculture (A)

- (b) The zoning reclassification of a portion of the Property is from its present zoning to:

Service Commercial (CS)

- (c) Notice to the Owner pursuant to Paragraph No. 4, shall be addressed to:

Mr. Will Dyck
2025 N. Gateway, Suite 101
Fresno, CA 93727

- (d) "Property" as used in this Agreement, includes:

APN No's. 081-020-063 and 081-020-068.

- (e) The additional conditions with which the Owner shall comply, pursuant to Paragraph No. 2, of this Agreement, are as follows:

That the Design Guidelines attached as Exhibit II apply to the subject parcels and any future parcels.

EXHIBIT II
CONDITIONAL ZONING AGREEMENT NO. 2003-01
Design Guidelines

**City of Visalia
Agenda Item Transmittal**

Meeting Date: September 2, 2003

Agenda Item Number (Assigned by City Clerk): 10

Agenda Item Wording: Second reading and adoption of Ordinance No. 2003-13 for the following:

Change of Zone No. 2002-18, an amendment to the Zoning Ordinance to change the zone from Agriculture (A) to Service Commercial (CS). The project is 72 acres located west of Plaza Drive between Mill Creek Ditch and Camp Drive (APN 081-02-063, 064, 068) to accommodate the Visalia Auto Plaza.

Applicant: Mangano Homes/Westland Development

Deadline for Action: None

Submitting Department: Community Development - Planning

Contact Name and Phone Number:

Darlene Mata, Principal Planner (559) 713-4002

For action by:

- ☒ City Council
- ☐ Redev. Agency Bd.
- ☐ Cap. Impr. Corp.
- ☐ VPFA

For placement on which agenda:

- ☐ Work Session
- ☐ Closed Session
- ☒ Regular Session:
- ☐ Consent Calendar
- ☐ Regular Item
- ☐ Public Hearing

Est. Time (Min.): 15 min

DEPARTMENT RECOMMENDATION AND SUMMARY:

Staff recommends that the City Council adopt Ordinance No. 2003-13 approving Change of Zone No. 2002-18 subject to Conditional Zoning Agreement No. 2003-01 attached.

Conditional Zoning Agreement 2003-01, attached contains Design Guidelines for the development of the Auto mall. The Planning Commission had recommended several changes to the Guidelines including the reduction of the maximum height of buildings from 50-feet to 35-feet and elimination of the provision to allow raised vehicle display pads.

Based on comments of the City Council, staff has not amended the guidelines as recommended by the Planning Commission. The primary goal in the guidelines is to provide common theme and design parameters for items such as the raised vehicle pads, signage and landscaping. The guidelines currently state that each dealership will be allowed two raised pads per manufacturer line to a maximum of six per parcel. For example, if a dealership sells only one car line, then only two raised pads would be allowed. If a dealership consists of two car lines then they would be allowed four. All the pads will be required to be constructed in conformance with a standard design. The pads will have a block or stone base a maximum height of three feet that will coordinate with the monument signs. The requirement that all raised pads, and monument signs, look the same will provide the continuity of design mentioned by the auto mall consultant at the public hearing.

Another amendment to the Design Guidelines is to signage. The applicant proposes two center identification signs, at the intersection of Hurley and Neeley Streets and Neeley and Crowley Street. The center identification signs will both be the same design with a low profile curved wall a maximum height of 5-feet with landscaping surrounding the sign. The signs will be

Prepared by Darlene Mata
Last Revised 08/28/2003

COUNCIL ACTION: Approved as Recommended

3-1 (56-NO)

Page 1

designed to coordinate with the monument signs in the development. The Design Guidelines also contain a provision that would allow two raised pads near the center identification sign at the Hurley and Neeley Street intersection. Staff has amended the Guidelines to state that the two raised pads will be allowed if the property owners association can provide for the joint use of the pads by all dealerships within the Auto mall. If a provision is not made for the joint use of the pads, they will not be allowed.

There was discussion at the Council meeting on limiting the use of the Auto mall to new car dealerships. The Guidelines have been clarified to state that uses within the project are limited to new car dealerships with ancillary used car sales and the normal service components, see page 4 in the Design Guidelines. No stand alone used car dealerships or service businesses will be allowed within the project area. Minor changes to the text were made in these areas only.

The Design Guidelines will become part of the Zoning Agreement to be recorded. . The City Council can direct staff to make any other changes to the guidelines deemed necessary to facilitate a quality development. Copies of the Zoning Agreement signed by the property owners are attached. There are currently two copies signed by the two owners, the final original to be signed by the City and recorded will have all the signatures on one document.

COMMITTEE/COMMISSION REVIEW AND ACTIONS:

On August 18, 2003, the City Council approved the resolutions and ordinance approving the general plan amendment and change of zone.

The Planning Commission approved a recommendation of approval of General Plan Amendment 2002-20 and Change of Zone 2002-18 on July 28, 2003.

EXHIBITS:

Ordinance No. 2003 – 13 with Zoning Agreement 2003-01

Recommended Motion (and Alternative Motions if expected):

1. I move to approve the second reading of Ordinance No. 2003-13 approving Change of Zone 2002-18 and Conditional Zoning Agreement No. 2003-01.

CITY MANAGER RECOMMENDATION:

Environmental Assessment Status

CEQA Review:

Required? Yes

Review and Action:

Prior:

Required

City Council certified the EIR on August 18, 2003

NEPA Review:

Required? No

Review and Action:

Prior:

Required

Financial Impact

Funding Source:

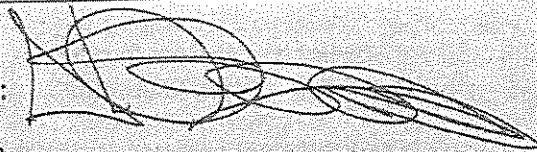
Account Number: _____ (Call Finance for assistance)

Budget Recap:

Total Estimated cost: \$	New Revenue: \$
Amount Budgeted: \$	Lost Revenue: \$
New funding required: \$	New Personnel: \$
Council Policy Change: Yes _____	No _____

Review and Approval - As needed:

Department Head Review (Signature):



Risk Management Review (Signature):

City Attorney Review (Signature):

Administrative Services Finance Review (Signature):

Others:

ORDINANCE NO. 2003-13

AMENDING A PORTION OF THE ZONING MAP FROM (A) AGRICULTURE TO (CS) COMMERCIAL SERVICE, LOCATED NORTH OF STATE HWY 198, WEST OF PLAZA DRIVE; MANGANO HOMES/WESTLAND DEVELOPMENT COMPANY, APPLICANT.

BE IT ORDAINED BY THE COUNCIL OF THE CITY OF VISALIA

Section 1: The City Council certified that the Final Environmental Impact Report for the Visalia Auto Plaza Project, SCH 2003021076, was prepared consistent with the California Environmental Quality Act (CEQA) and City of Visalia Environmental Guidelines on August 181, 2003;

Section 2: The City Council adopted a Mitigation Monitoring Program to monitor the implementation of mitigation measures identified to reduce impacts to a level of less than significant and a Statement of Overriding Considerations for those impacts which could not be mitigated to a level less than significant.

Section 3: The Planning Commission of the City of Visalia has recommended to the City Council that property designated (A) Agriculture, located north of State HWY 198, west of Plaza Drive, be rezoned to CS (Commercial Service).

Section 4: The amendment is subject to provisions of Conditional Zoning Agreement No. 2003-01, attached hereto.

Section 5: This property is hereby zoned CS (Commercial Service) and Zoning Map of the City of Visalia is amended to show said parcel zoned CS (Commercial Service).

Section 6: This ordinance shall become effective 30 days after passage hereof or upon execution of Conditional Zoning Agreement No. 2003-01, whichever is later.

PASSED AND ADOPTED: 9/2/03

ATTEST:

STEVEN M. SALOMON, CITY CLERK

JESUS J. GAMBOA, MAYOR

Jesus J. Gamboa

APPROVED BY CITY ATTORNEY

Daniel M. Deoley

STATE OF CALIFORNIA)
COUNTY OF TULARE) ss.
CITY OF VISALIA)

I, Steven M. Salomon, City Clerk of the City of Visalia, certify the foregoing is the full and true Ordinance 2003-13 passed and adopted by the Council of the City of Visalia at a regular meeting held on September 2, 2003 and certify a summary of this ordinance has been published in the Visalia Times Delta.

Dated: *September 22*, 2003

STEVEN M. SALOMON, CITY CLERK

By *Roxanne Yoder*, Chief Deputy

NO FEE REQUIRED PURSUANT
TO GOVT. CODE SECTION 27383
RECORDING REQUESTED BY
AND MAIL RESPONSE TO:

City of Visalia
Planning Division
315 East Acequia
Visalia, CA 93291

CONDITIONAL ZONING AGREEMENT NO. 2003-01

THIS CONDITIONAL ZONING AGREEMENT NO. 2003-01 (the "Agreement") made this ____ day of ____ 2003, by American Property Holdings, I.L.C., a California limited liability company, hereinafter called "Owner" and the CITY OF VISALIA, a municipal corporation and charter law city of the State of California, hereinafter called, "City."

WITNESSETH

WHEREAS, the Owner is the owner of real property, herein called the "Property" situated in the City of Visalia, which Property is described in Item (d) of Exhibit I of this Agreement; and

WHEREAS, the Property is now zoned as specified in Item (a) of Exhibit I; and

WHEREAS, Mangano Homes / Westland Development, agent of the Owner, have applied for a rezoning of the Property pursuant to which application the Property is being rezoned from its present classification to the classification or classifications specified in Item (b) of Exhibit I; and

WHEREAS, hearings have been held upon said application before the City Council of the City of Visalia, State of California, and after having considered the matter presented, it has been determined that certain conditions to the rezoning of said Property must be imposed so as not to endanger the health, safety, and the general welfare of the City of Visalia and its residents.

NOW, THEREFORE, IT IS MUTUALLY UNDERSTOOD AND AGREED that inasmuch as the rezoning specified in Item (b) of Exhibit I is being granted, the rezoning shall be subject to the conditions specified in the following paragraphs:

1. That Exhibit I, as completed and attached hereto, is incorporated into and made a part of this Agreement with the same force and effect as if fully set forth herein.
2. That the Owner shall comply with the additional conditions specified in Item (e) of Exhibit I of this Agreement.
3. In the event the Owner, any successor in interest of the Owner, or any person in possession of the Property described in Item (d) of Exhibit I violates or fails to perform any of the conditions of this Agreement within thirty (30) days after notice thereof as provided in Paragraph 4, the City may institute legal proceedings to enforce the provisions of this Agreement. Any failure by the Owner to perform or comply with any of the conditions under Item (e) of Exhibit I of this Agreement within thirty (30) days after written notification from City to do so, shall constitute a default in this Agreement by the Owner. In addition to any other remedy it may have at law or equity, City may, but is not obligated to, perform any such conditions or requirements set forth in Item (e) of Exhibit I of this Agreement either through its own employees or through parties with whom it may contract, and assess the full costs and expenses thereof to the Owner. The

Owner shall reimburse City said costs and expenses within thirty (30) days after a written invoice from the City.

4. The prevailing party in any legal action arising from the failure of either party to perform the duties and obligations required by and contained in this Agreement shall be entitled to reasonable attorney's fees, expert witness fees and costs of suit.
5. Notice of violation of provisions of this Agreement shall be sent to the Owner at the address specified in Item (c) of Exhibit I. Any subsequent title holder, any lien holder, or party in possession of the Property shall also receive notice if they are reflected on the tax roll.
6. Each and every one of the provisions of this Agreement herein contained shall run with the land and shall bind and inure to the benefit of the successors in interest, heirs and assigns, in the same manner as if they had herein been expressly named.
7. Zoning of the Property as indicated in Item (b) of Exhibit I shall not be consummated until such time as the Agreement has been recorded in the office of the Tulare County Recorder.
8. This Agreement is the product of negotiation and compromise on the part of each party and the parties agree, notwithstanding Civil Code § 1654, that in the event of uncertainty the language will not be construed against the party causing the uncertainty to exist.
9. This Agreement may be amended by mutual consent if in writing and, in the case of City, authorized by its City Council.

IN WITNESS WHEREOF, the parties hereto have executed this Agreement the day and year first above written.

"Owner"

American Properties Holdings, LLC,
a California limited liability company

Land Securities Investors, LTD.,
a Colorado limited partnership

By: Sunset Management Services, Inc.
a Colorado Corporation, General Partner
By: Alan R. Fishman
Alan R. Fishman, President

"City"

City of Visalia,
a municipal corporation

By: _____
City Manager

ATTEST: _____
Clerk of the City Council

Velma Dyck

EXHIBIT I
CONDITIONAL ZONING AGREEMENT NO. 2003-01

- (a) The Property prior to the rezone contemplated in this agreement is now zoned:

Agriculture (A)

- (b) The zoning reclassification of a portion of the Property is from its present zoning to:

Service Commercial (CS)

- (c) Notice to the Owner pursuant to Paragraph No. 4, shall be addressed to:

Mr. Will Dyck
2025 N. Gateway, Suite 101
Fresno, CA 93727

- (d) "Property" as used in this Agreement, includes:

APN No's. 081-020-063 and 081-020-068.

- (e) The additional conditions with which the Owner shall comply, pursuant to Paragraph No. 2, of this Agreement, are as follows:

That the Design Guidelines attached as Exhibit II apply to the subject parcels and any future parcels.

NO FEE REQUIRED PURSUANT
TO GOVT. CODE SECTION 27383
RECORDING REQUESTED BY
AND MAIL RESPONSE TO:

City of Visalia
Planning Division
315 East Avequia
Visalia, CA 93291

CONDITIONAL ZONING AGREEMENT NO. 2003-01

THIS CONDITIONAL ZONING AGREEMENT NO. 2003-01 (the "Agreement") made this 16 day of Aug, 2003, by American Property Holdings, LLC, a California limited liability company, hereinafter called "Owner" and the CITY OF VISALIA, a municipal corporation and charter law city of the State of California, hereinafter called, "City."

WITNESSETH

WHEREAS, the Owner is the owner of real property, herein called the "Property" situated in the City of Visalia, which Property is described in Item (d) of Exhibit I of this Agreement; and

WHEREAS, the Property is now zoned as specified in Item (a) of Exhibit I; and

WHEREAS, Mangano Homes / Westland Development, agent of the Owner, have applied for a rezoning of the Property pursuant to which application the Property is being rezoned from its present classification to the classification or classifications specified in Item (b) of Exhibit I; and

WHEREAS, hearings have been held upon said application before the City Council of the City of Visalia, State of California, and after having considered the matter presented, it has been determined that certain conditions to the rezoning of said Property must be imposed so as not to endanger the health, safety, and the general welfare of the City of Visalia and its residents.

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1. That Exhibit I, as completed and attached hereto, is incorporated into and made a part of this Agreement with the same force and effect as if fully set forth herein.
2. That the Owner shall comply with the additional conditions specified in Item (e) of Exhibit I of this Agreement.
3. In the event the Owner, any successor in interest of the Owner, or any person in possession of the Property described in Item (d) of Exhibit I violates or fails to perform any of the conditions of this Agreement within thirty (30) days after notice thereof as provided in Paragraph 4, the City may institute legal proceedings to enforce the provisions of this Agreement. Any failure by the Owner to perform or comply with any of the conditions under Item (e) of Exhibit I of this Agreement within thirty (30) days after written notification from City to do so, shall constitute a default in this Agreement by the Owner. In addition to any other remedy it may have at law or

equity, City may, but is not obligated to, perform any such conditions or requirements set forth in Item (c) of Exhibit I of this Agreement either through its own employees or through parties with whom it may contract, and assess the full costs and expenses thereof to the Owner. The Owner shall reimburse City said costs and expenses within thirty (30) days after a written invoice from the City.

4. The prevailing party in any legal action arising from the failure of either party to perform the duties and obligations required by and contained in this Agreement shall be entitled to reasonable attorney's fees, expert witness fees and costs of suit.
5. Notice of violation of provisions of this Agreement shall be sent to the Owner at the address specified in Item (c) of Exhibit I. Any subsequent title holder, any lien holder, or party in possession of the Property shall also receive notice if they are reflected on the tax roll.
6. Each and every one of the provisions of this Agreement herein contained shall run with the land and shall bind and inure to the benefit of the successors in interest, heirs and assigns, in the same manner as if they had herein been expressly named.
7. Zoning of the Property as indicated in Item (b) of Exhibit I shall not be consummated until such time as the Agreement has been recorded in the office of the Tulare County Recorder.
8. This Agreement is the product of negotiation and compromise on the part of each party and the parties agree, notwithstanding Civil Code § 1654, that in the event of uncertainty the language will not be construed against the party causing the uncertainty to exist.
9. This Agreement may be amended by mutual consent if in writing and, in the case of City, authorized by its City Council.

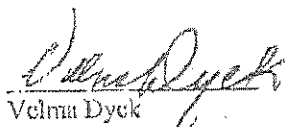
IN WITNESS WHEREOF, the parties hereto have executed this Agreement the day and year first above written.

"Owner"

American Property Holdings, LLC,
a California limited liability company

Land Securities Investors, LTD.,
a Colorado limited partnership

By: Sunset Management Services, Inc.
a Colorado Corporation, General Partner
By: _____
Alan R. Fishman, President


Velma Dyck

"City"

City of Visalia,
a municipal corporation

By: _____
City Manager

ATTEST: _____
Clerk of the City Council

EXHIBIT I
CONDITIONAL ZONING AGREEMENT NO. 2003-01

- (a) The Property prior to the rezone contemplated in this agreement is now zoned:

Agriculture (A)

- (b) The zoning reclassification of a portion of the Property is from its present zoning to:

Service Commercial (CS)

- (c) Notice to the Owner pursuant to Paragraph No. 4, shall be addressed to:

Mr. Will Dyck
2025 N. Gateway, Suite 101
Fresno, CA 93727

- (d) "Property" as used in this Agreement, includes:

APN No's. 081-020-063 and 081-020-068.

- (e) The additional conditions with which the Owner shall comply, pursuant to Paragraph No. 2, of this Agreement, are as follows:

That the Design Guidelines attached as Exhibit II apply to the subject parcels and any future parcels.

VISALIA AUTO PLAZA

Architectural Design Guidelines

August 19, 2003

Prepared By:

QK4

Architecture.Engineering.Construction.

TABLE OF CONTENTS

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INTRODUCTION

Visalia Auto Plaza (VAP) is a 70 acre, state of the art, regional automotive shopping and service experience for customers in the greater Visalia / Tulare / Hanford area. Modeled after retail concepts of the regional shopping mall, the VAP provides shoppers a unique destination where multiple vehicle dealerships are consolidated on one property of facilitate their purchase and service needs. Variety and selection are the two primary goals of any shopping experience and the VAP through the introduction of separate vehicles brands on properties ranging from approximately 4 to 13.5 acres.

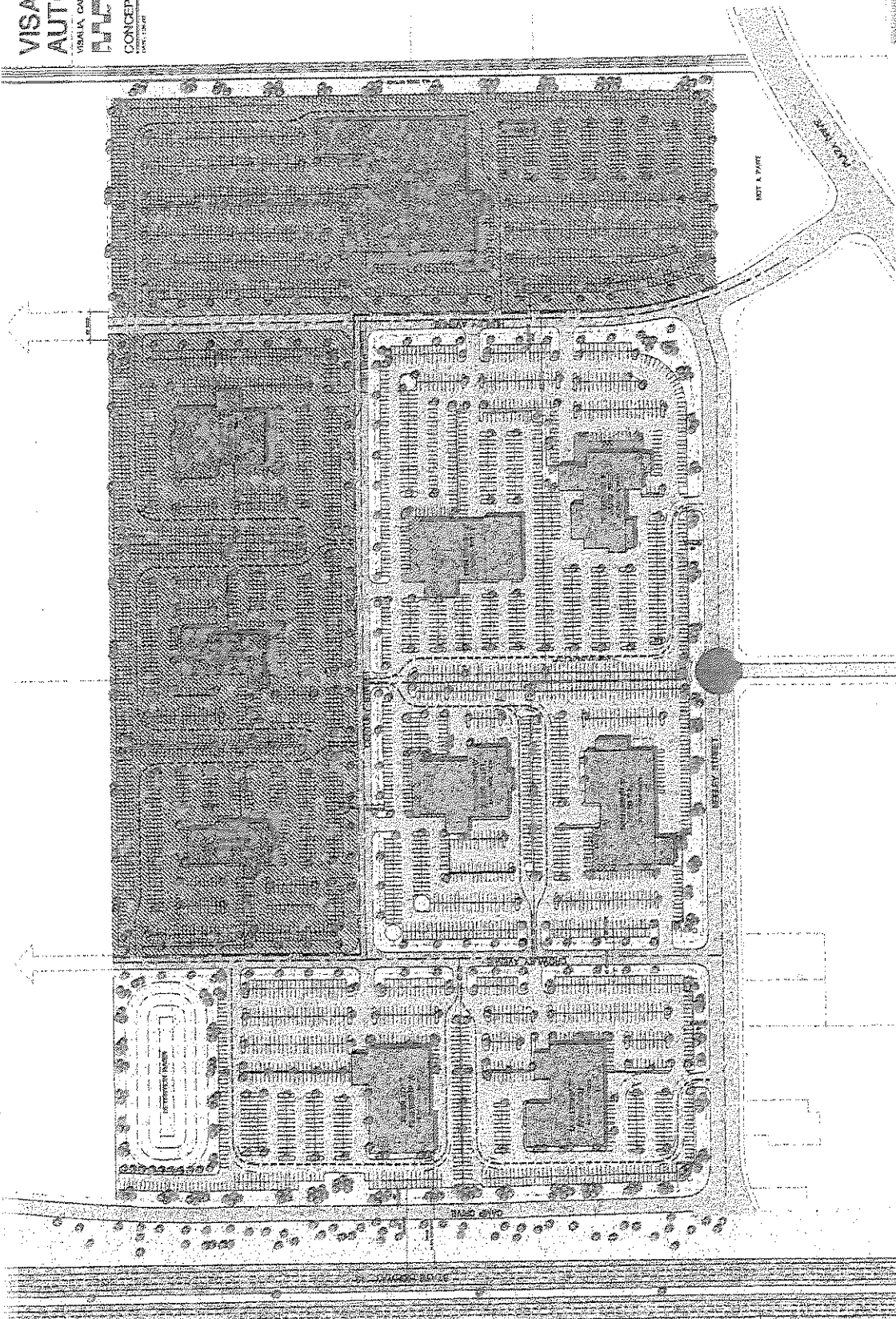
The following architectural design standards have been created as guidelines for the development of the auto plaza and to create continuity between each facet of the project as it is designed and constructed.

SITE MASTER PLAN

The overall Site Master Plan for the Visalia Auto Plaza is shown with all of the proposed phases, is reduced and shown on the next page, Exhibit 1.

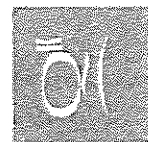
SITE IMPROVEMENT

Site improvements will be constructed as each individual dealership develops. These improvements will include construction of ½ streets, plus a 12 foot wide travel lane as well as frontage improvements such as curb and gutter, landscaping and street lighting along the project side only.



Autopilot

425 S. 4th Street
 Suite 101
 Visalia, CA 93291
 Tel. 419-377-2723
 Fax. 419-377-2230
 www.934.com



SITE PLANNING CONCEPTS

- A. Building location and open spaces; Visalia Auto Plaza auto mall is planned on a parcel of land that is within the gateway to the City of Visalia along State Highway 198. Careful consideration was given to the appearance and site lines from the highway. The building orientation and land uses that abut the highway were purposely laid out to enhance this scenic corridor. Therefore the intent was not to back the project up to the highway, but to open it up and allow site lines with views into the center. To enhance the concept, peripheral building elements (landscape, hardscape, screen walls, sign bases, etc.) will have unifying architectural styles. Buildings will be built on stand-alone pads.
- B. Proposed uses; Uses within the project and restricted to new car dealerships with ancillary used car sales and normal service components. No stand alone used car facilities will be allowed in the project.
- C. Access; Access to the Auto Plaza will be from Plaza Drive. Ingress and egress for individual dealerships will be from Neeley, Hurley, Century, and Crowley Streets. No access to individual parcels will be permitted from Camp Drive.
- D. Phasing of Construction; The project will be constructed in two phases. The developer will construct all improvements to the public streets, underground utilities, common area landscaping, and signage at each phase. Construction of improvements for each parcel would be the responsibility of individual parcel owner. Each individual parcel will provide the required parking for their building. Site drainage will be designed so that each phase will work independently of the other phases.

REQUIRED SETBACKS

Building and landscape setbacks are required and will be established through the Architectural Design Guidelines. Setbacks are to be measured from the edge of the property boundary unless otherwise indicated.

A. Building Setbacks:

- | | |
|-------------------------|---|
| • Hurley, Camp & Neeley | 30' Minimum |
| • Century, & Crowley | 15' Minimum |
| • Mill Creek | 5' Minimum from edge of Conservation Easement |
| • Side Yard | 5' Minimum |
| • Rear Yard | 5' Minimum |

B. Landscaping Setback:

- Camp, Hurley & Neeley 30' Minimum
- Century, Crowley 15' Minimum
- Mill Creek Ditch 5' Minimum from edge of Conservation Easement
- Side Yard 5' Minimum
- Rear Yard 5' Minimum

C. Building Coverage:

- Each site shall have a maximum building coverage of fifty (50) percent of the net site area.

D. Building Height:

- Showroom building height shall be to a maximum of thirty (30) feet. The height shall be measured from the finished surface grade level to the highest point of the roof.

PARKING REQUIREMENTS

A. Customer and employee parking shall be incorporated on the site plans and shall clearly be identified by on-site signage.

B. Minimum parking requirements are as follows:

- Automobile dealerships; One parking space for each two employees during the time of maximum employment, plus one parking space for each two thousand (2,000) square feet of lot and building area used for the display or storage of automobiles.
- No parking of vehicles for sale will be allowed in the public right of way or landscape setbacks (with the exception of raised dealer display pads)

C. Loading and Unloading Facilities:

- Each Dealership site shall provide a loading and unloading area which shall be a minimum of twelve (12) feet by 105 feet. This special area shall be designated on the site plan and shall be in conjunction with the inventory storage area. The design of the loading and unloading area shall provide for the following:
- Incorporate ease of ingress and egress for safety and efficiency.
- The loading and unloading facilities shall not adversely interfere with on-site circulation.

- The loading and unloading facilities shall be designed to accommodate vehicle carrier truck maneuvers on site, without backing from or into a public street.

DESIGN REVIEW / SUBMITTAL PROCESS / GENERAL ARCHITECTURE

A design review and submittal process has been established to ensure that all elements of the development within the Visalia Auto Plaza meet the requirements set forth in these design guidelines. All plans and specifications will be submitted through City of Visalia's Site Plan Review process.

- A. No building, exterior signs or structures shall be erected or exterior alterations or additions made on any site unless such improvements are in conformance with the plans and specifications approved by the Architectural Review Committee. The approval shall encompass landscaping and architecture as specified by these standards.
- B. The Architectural Review Committee shall be established prior to development of the auto dealership and will consist of the following members:
 - Two (2) Developer representatives – Auto Plaza Associates, LLC
 - Two (2) Dealership representatives –
 - One (1) Architectural representative –
 - One (1) City representative as appointed by the City Planner -
- C. Processing through the Auto Plaza Architectural Review Committee will be in addition to the reviews and submittals required by the City of Visalia. Approval or disapproval shall be made in writing within 30 days of application.
- D. Submittals to the Architectural Review Committee shall include:
 - A Site Plan showing the location of all of the buildings, landscaped areas, on-site parking areas, vehicular and pedestrian access ways, accessory structures and signage.
 - Drawings shall be submitted showing elevations of all buildings from all ground level views together with landscaping, accessory structures, building colors and rim materials.
 - A color board of all the surface materials of the buildings and accessory structures.
 - All plans shall be prepared by licensed, registered personnel appropriate to the designs being submitted.

- Additional supporting material shall be submitted which adequately demonstrate, to the satisfaction of the Architectural Review Committee, that the provisions of these guidelines are met.
- Landscaping plans shall be submitted which show plant selection and usage, conceptual grading, drainage and irrigation improvements.

E. Architectural Theme:

- The Visalia Auto Plaza intends to encourage a Modern/Contemporary architectural theme. The intent of these standards is not to limit innovative design approaches, nor is it to have all dealerships look exactly alike. Manufacturer image programs are acceptable.
- All buildings should be simple in form and should emphasize geometric shapes and horizontal line qualities. The use of curved surfaces is encouraged.

F. Colors:

- The color scheme shall emphasize earth tones and warm gray shades. The buildings' body color that is selected shall be used through each individual dealership and shall work to integrate all elements of the individual dealerships.

G. Building Materials Acceptable:

- Concrete masonry units
- Stucco
- Exposed timbers
- Precast concrete tilt-up panels.
- Ornamental iron
- New brick of rust color
- Flush surface metal panels

H. Buildings Materials Unacceptable:

- Exposed wood siding
- Corrugated metal siding
- Unpainted metal trim or flashing
- Shake or shingle roof

I. Building Orientation and Views:

- The dealership showrooms and exterior covered areas shall be oriented toward Hurley, Century, and Crowley Streets. Public entrances should be identified by color or form.
- Building and exterior display areas should be sited to complement existing topography, site configuration and adjacent uses. Onsite grading should be kept at a minimum.
- Service bays and repair areas are to be enclosed and located to the rear of the site area. Loading areas, storage areas and trash collection areas should be screened from view from public streets, pedestrian walks or common customer parking areas. The screen wall should be a minimum of six (6) feet in height and shall be masonry concrete or stucco construction consistent with the buildings materials used on the main building. Site fencing, where such fencing is not visible from public streets, may be chain link to maximum height of seven (7) feet with an eighteen (18) inch spiral razor barbed wire cap. No fencing shall be permitted in the front yard setback.
- Roof mounted mechanical equipment, ductwork, vents and access ladders shall be screened from view from the public streets, State Highway 198, pedestrian walks and common parking areas. The equipment shall be screened by the building from itself using parapets, roofs, etc. Individual equipment screens are not encouraged unless they are determined to be consistent with the general building design and materials by the Architectural Review Committee.
- Views from Highway 198; Six (6) foot screen walls will be used to minimize views of the service bays from the highway.
- Large wall masses are to be broken-up through the use of a combination of varying colors and or materials.

J. Vehicle Display Pads:

- Each parcel may have, except on Camp Drive frontages, two (2) raised vehicle display pads for each manufacturer line to a maximum of six (6) per parcel. All display pad shall be constructed consistent with the design in Exhibit 2 and may be located in the required landscape area a minimum of five (5) from property line and shall not create a site distance impact to vehicle traffic when exiting. Suggested distance from vehicle access drives is twenty-five (25) feet.
- Vehicle Display pads may also be used to enhance the Visalia Auto Plaza Center Identification Sign Area as shown in Exhibit 3 if the property owner/dealership association includes a mechanism for the sharing of these pads by all the dealerships on a rotating basis. Otherwise, raised pads are not allowed within 50 feet of the center identification signs.

Exhibit 2 – Raised Pads

Exhibit 3 – Center ID Sign

LANDSCAPING

The primary objective of the landscape guidelines is to promote a visual separation between the dealers, parking lots and adjacent properties. The goal is to promote an environment within the auto mall that is pleasing, inviting, and unified.

A. Landscaping:

- All landscaping will be in accordance with section 17.30.130 of the City of Visalia's Zoning Ordinance, with a provision for clustering of trees.
- Every site on which a building is placed shall be landscaped and maintained thereafter in a well kept condition. A landscaping plan shall be submitted to the Architectural Review Committee. Such landscaping plan shall be prepared by a landscape architect licensed in the State of California.
- Interior landscaping shall be utilized to break up large paved areas and soften building lines.
- The thirty (30) foot front landscaping shall be gently mounded to maximum height of eighteen (18) inches utilizing a turf or ground cover treatment. Trees and shrubs shall be used at common property lines to define site separations.
- The Mill Creek Conservation Easement will be landscaped with riparian plantings.

B. Soil Preparation:

- To ensure proper root growth, soil tests shall be performed and soil additives used to provide adequate nutrients, rooting environmental and soil Ph.

C. Irrigation:

- All landscaped planting areas shall be fully irrigated with complete coverage. All systems/circuits shall be underground. Detailed landscaping and irrigation plans shall be submitted to the City of Visalia Planning Department for review and approval prior to issuance of building permits.
- All irrigation systems shall be automatically controlled with electromechanical or solid state controllers able to have variable length timing circuits.
- Water conservation systems are recommended.
- All valves (remote control valves, quick coupler valves, shut-off valves, etc.) shall be installed in valve boxes which are flush to finished landscaped grade.

D. Fencing:

- On-site fencing may include a chain fence to a maximum total height of seven (7) feet with an optional eighteen (18) inch spiral razor ribbon barbed wire cap, where such fencing is not visible from the public right of way, public walkways and where it does not extend into the thirty (30) foot front yard setback. Masonry walls of the same height may be substituted at owner's option, or where required by the City of Visalia. This requirement is designed to ensure security for individual properties.

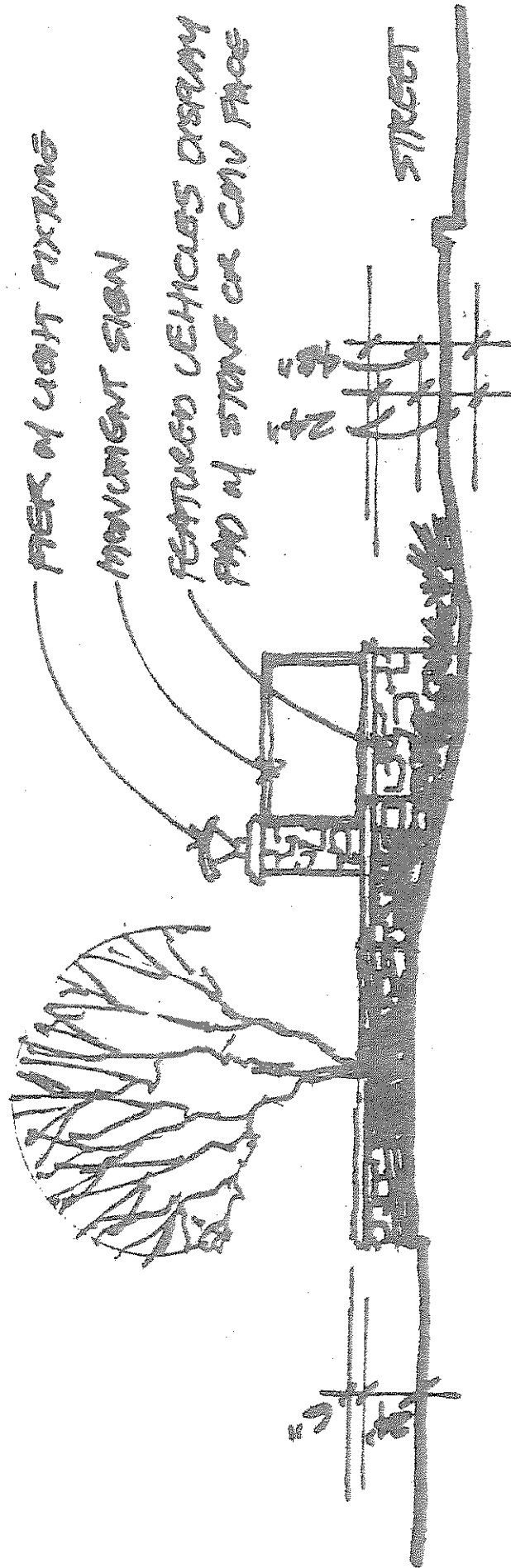
SIGNAGE

A. General Requirements

- Each dealership shall be allowed only those signs that are necessary for identification of the franchise, the premises, the department uses on site and directional signing. The design size, location, color and materials of construction shall be submitted to the Architectural Review Committee for approval prior to construction. All dealerships shall comply with the City of Visalia Sign Ordinance except as provided herein. Billboards, streamers, temporary signs, revolving signs, flashing signs and moving signs are specifically prohibited.
- No signs visible from outside the property may be placed, parked, attached to or displayed from the site except as provided herein. No signs projecting from the roof line of any building or painted on the sides of buildings or roof without approval of the Architectural Review Committee.
- No temporary signs or advertising materials such as balloons, streamers, flags, and inflatable advertising shall be permitted.
- Center identification signs shall only be allowed at the southwest corner of Hurley and Neeley Streets and the T-intersection of Crowley and Neeley as shown in Exhibit 3.

B. Free Standing Monument Signs

- Each dealership shall be allowed one double faced monument sign for each auto line to a maximum of two per parcel consistent with the sign design in Exhibit 4. The sign shall be located on the primary street frontage. Such sign shall be ground-mounted containing the dealership name and manufacture name of the new automobiles sold on the site. The maximum height of the sign shall not exceed ten (10) feet. The total illuminated sign area shall not exceed thirty-five (35) square feet of sign copy per face. The total aggregate surface area of the sign faces and the structure shall not exceed one hundred forty (140) square feet.



MONUMENT SIGN DETAIL

- Freestanding monument signs shall be setback a minimum of five (5) feet from front property lines and a minimum of twenty (20) feet from any interior side property line.
- Freestanding signs are not allowed on the Camp Drive frontage.

C. Building Signs

- Each dealership shall be allowed building signage identifying the dealership name and manufacture name of new automobiles sold on the site. The sign(s) shall be located on the top showroom fascia facing the primary street or on a flat wall surface. The signs shall be preformed, individual illuminated letters with Plexiglas faces in deep channel cans. The maximum height of the letters shall be thirty-six (36) inches. Logo signs shall not exceed ten (10) square feet in area.
- Building signs along Camp Drive shall be limited to secondary signage only.
- Primary Sign - Two square feet of sign area is permitted for each foot of linear occupancy frontage to a maximum of one hundred fifty (150) square feet. Dealerships may choose which exterior side of the building will be used for the purpose of calculating the permitted sign area.
- Secondary Sign - Additional signs of a maximum of twenty-five (25) percent of the sign area as calculated for the primary sign shall be allowed for each remaining exterior wall provided that the sign area for any given wall shall not exceed two square feet per linear foot of the wall length.
- Variances to building sign area may be approved by the Planning Commission in accordance with Section 17.48.110 of the City of Visalia Zoning Ordinance. Approval of a variance shall be based on providing adequate signage for all manufacturer lines at a single dealership.

D. Department Identification Signs

- One wall sign is allowed for each department (e.g., use card, service, parts, body shop, etc). The maximum height of such signs shall be eight (8) inches with a maximum total area of three square feet per sign.

E. Directional signs

- Dealership directional signs shall be limited to "Enter", "Exit", "Service Entrance", "Customer Parking", and "Employee Parking". Sign height shall not exceed four (4) feet with a maximum sign area of four (4) square feet.

LIGHTING

Illumination of the auto mall is important for the safety and security of the pedestrian, and motorist. The quality of the lighting needs to be maintained throughout the center and reinforced through the consistent use height, spacing, color and type of fixture used with in the area.

- Although lighting plans for the auto mall have not yet been developed, a general concept for project lighting to be employed in the phase I development has been established. The plan for night lighting for phase I development would provide for sufficiently bright lighting for security and safety purposes, but would avoid offensive glare and direct illumination of off-site locations. This would be accomplished by specifying the use of metal halide lighting in the parking lots, display areas, and internal circulation areas, by recessing light source completely within their fixture, and through the use of cut-off shields on light standards to block direct illumination beyond the parcel boundaries.
- Search Lights (temporary or permanent) are prohibited.



SITE INFORMATION PHASE 1			
ACREAGE			AC
CARMAX DEVELOPED:		5.01	AC
DETENTION:		0.00	AC
WETLANDS:		0.00	AC
BUFFERS:		0.69	AC
EASEMENTS:		0.00	AC
EXPANSION:		0.21	AC
SURPLUS:		0.00	AC
TOTAL:		5.91	AC
BUILDING INFORMATION			
TYPE:		"XF"	
SALES:		4,292	SF
PRESENTATION:		784	SF
RETAIL SERVICE:		2,620	SF
PRODUCTION:		0	SF
CARWASH:		923	SF
FQC:		0	SF
OTHER:		0	SF
TOTAL:		8,673	SF
PARKING	PROVIDED		
SALES LOT:	248	1.58	AC
CUST./EMP:	86	1.08	AC
N-S:	150	1.20	AC

BOL		FOUND MONUMENT
BSW		BOLLARD
CR		BACK OF SIDEWALK
		CROWN
		DOMES, DETECTABLE WARNING SURFACE
ELECPAN		ELECTRICAL PAN
EP		EDGE OF PAVEMENT
		EXISTING FENCE, CHAIN LINK
FH		EXISTING FENCE, WROUGHT IRON
		EXISTING FIRE HYDRANT
FL		EXISTING REALTOR SIGN
		FLOWLINE
FSW		FRONT OF SIDEWALK
	-(G)-	EXISTING GAS LINE, APPROXIMATE LOCATION
HP		HINGE POINT
IVB		IRRIGATION VALVE BOX
OG		ORIGINAL GROUND
	-(SD)-	EXISTING STORM DRAIN LINE, APPROXIMATE LOCATION
SDOI		STORM DRAIN INLET
SDMH		STORM DRAIN MANHOLE
	-(SS)-	EXISTING SANITARY SEWER LINE, APPROXIMATE LOCATION
SSMH		EXISTING SANITARY SEWER MANHOLE
TC		TOP OF CURB
TREE		TREE
	-(W)-	EXISTING WATER LINE, UNDERGROUND, APPROXIMATE LOCATION
WV		WATER VALVE
		EXISTING PROPERTY LINE
		EXISTING PUE
		PROPOSED MASONRY WALL
		PROPOSED VERTICAL C&G
		PROPOSED ROLL-OVER C&G
		PROPOSED SWALE
		PROPOSED FENCE, GUARD RAIL
		PROPOSED FENCE, ORNAMENTAL
	-(SS)-	PROPOSED SEWER LINE
		PROPOSED SEWER CLEANOUT
		PROPOSED SEWER MANHOLE
	-(SD)-	PROPOSED STORMDRAIN LINE
		PROPOSED STORMDRAIN INLET
		PROPOSED STORMDRAIN MANHOLE
	-(W)-	PROPOSED WATER LINE
	-(FW)-	PROPOSED WATER LINE FOR FIRE SERVICES
	-(UE)-	PROPOSED UNDERGROUND, ELECTRIC LINE
		PROPOSED PROPERTY LINE
	-(G)-	PROPOSED GAS LINE

PROPOSED STANDARD PAVEMENT

PROPOSED HEAVY DUTY PAVEMENT AND FIRE LANE

PROPOSED CONCRETE SURFACE

NEW R1T3 (CA) SIGN
NEW STREET LIGHT, OLA
NEW STREET LIGHT, OLB
NEW STREET LIGHT, OLC
NEW STREET LIGHT, OLG
NEW STREET LIGHT, OLW
CITY STANDARD COLLECTOR STREET LIGHT

NOT FOR
CONSTRUCTION



REUSE OF DOCUMENT
THE IDEAS & DESIGN INCORPORATED HEREON, AS
AN INSTRUMENT OF PROFESSIONAL SERVICE, IS
THE PROPERTY OF CENTERPOINT ENGINEERING &
IS NOT TO BE USED FOR ANY OTHER PROJECT
WITHOUT WRITTEN AUTHORIZATION OF
CENTERPOINT ENGINEERING.

SCALE VERIFICATION
 BAR IS ONE INCH ON ORIGINAL DRAWING
 [Redacted]
 IF NOT ONE INCH ON THIS SHEET ADJUST SCALES

[illegible]

ARmax ---
STORE NO.: 6064

**HILLSDALE AVENUE
VISALIA, CA**

PROJECT #	210166
DATE	11/10/21
DRAWN BY	AG, AL
QA-QC BY	PDM
SCALE	AS SHOWN

SITE PLAN

SHEET # **1 OF 4**



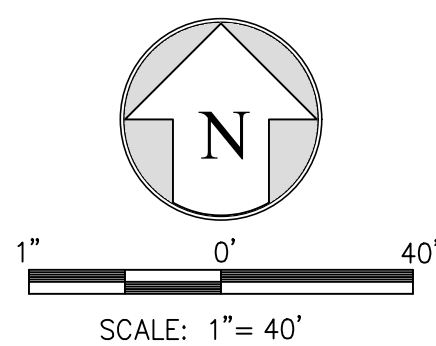


SCALE VERIFICATION
BAR IS ONE INCH ON ORIGINAL DRAWING
IF NOT ONE INCH ON THIS SHEET ADJUST SCALES
ACCORDINGLY

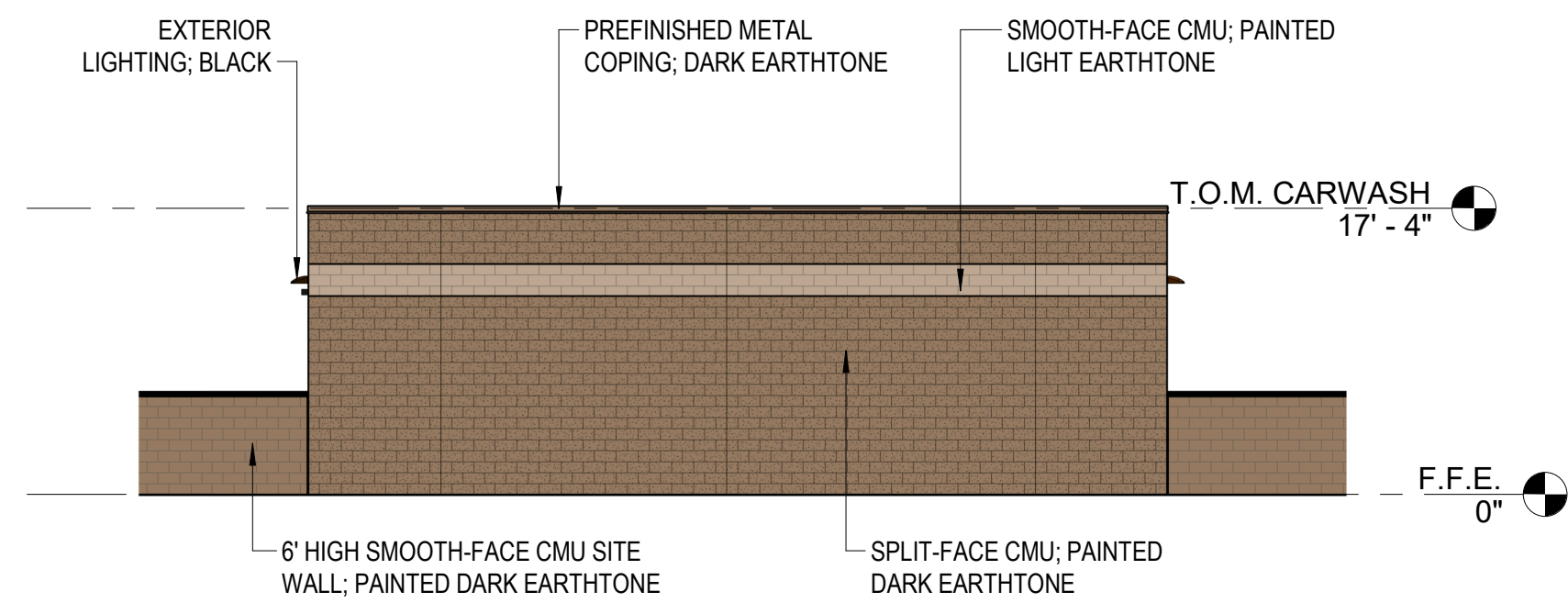
CARmax ---
STORE NO.: 6064
HILLSDALE AVENUE
VISALIA, CA

UTILITY PLAN

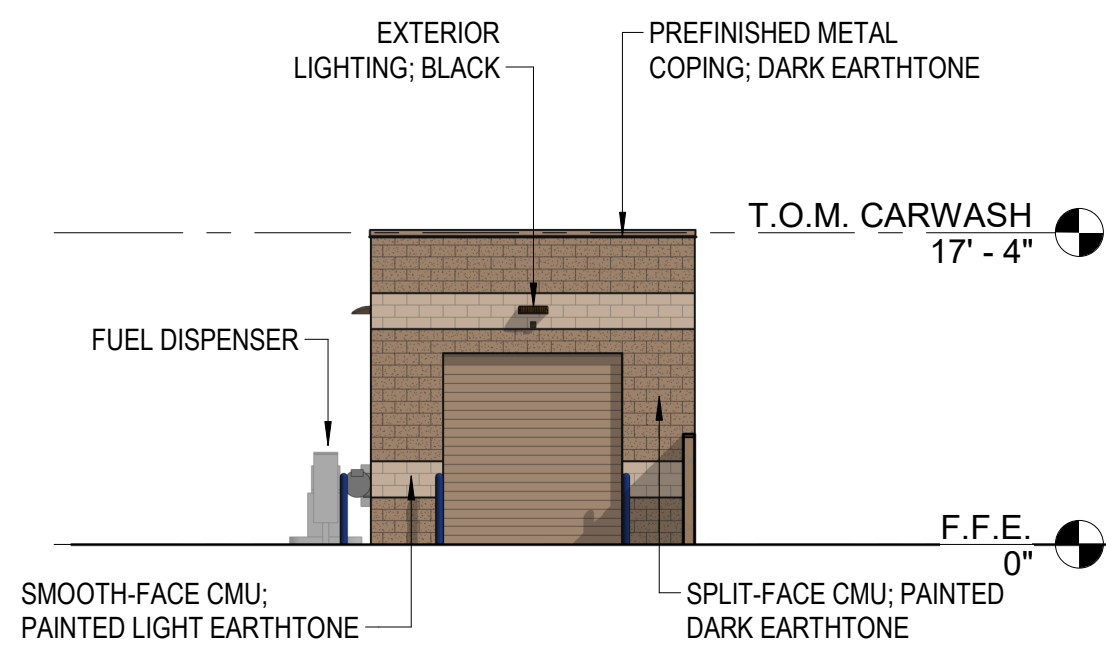
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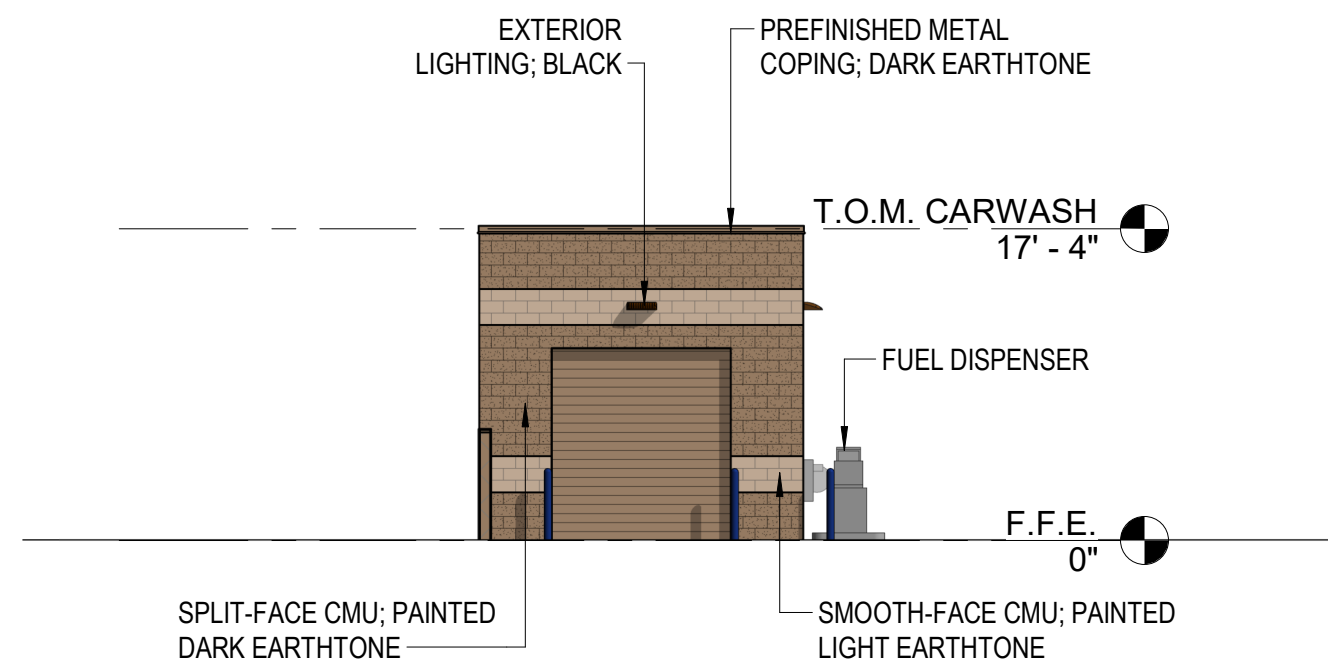
HIGHWAY 198



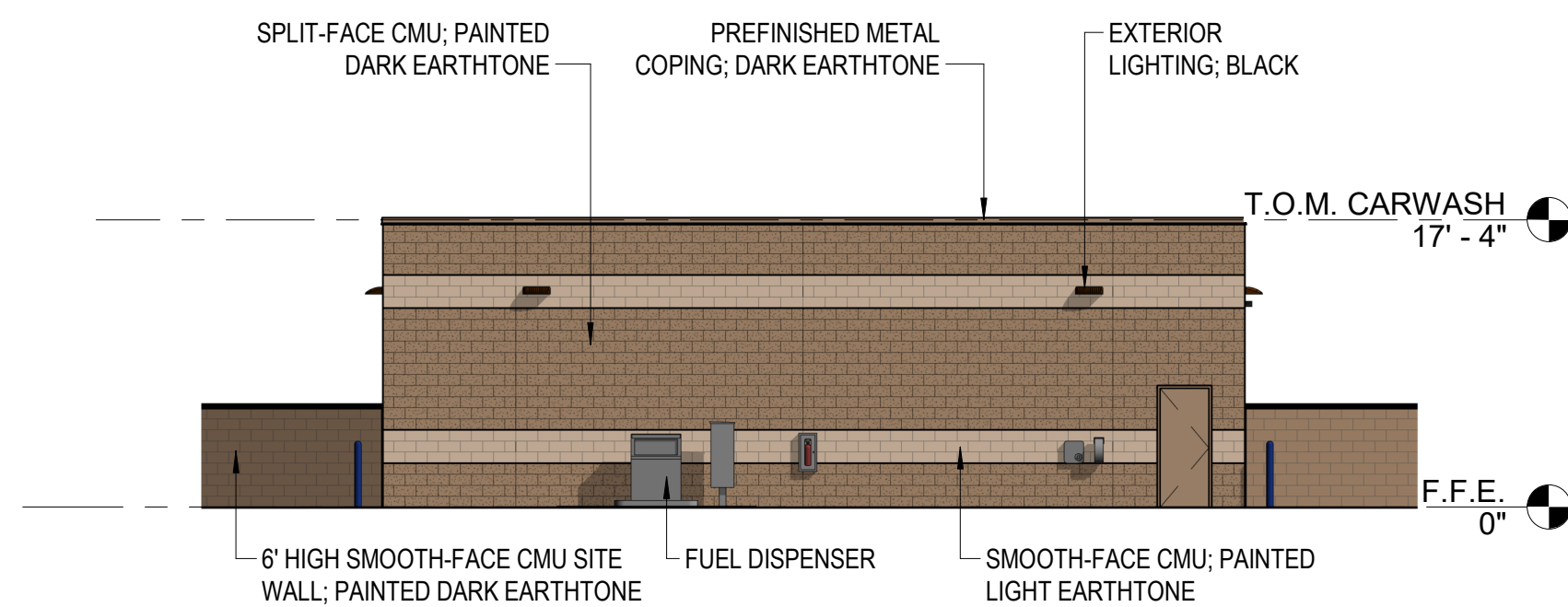
5 WEST ELEVATION - CARWASH
A 3.0 3/32" = 1'-0"



6 NORTH ELEVATION - CARWASH
A 3.0 3/32" = 1'-0"



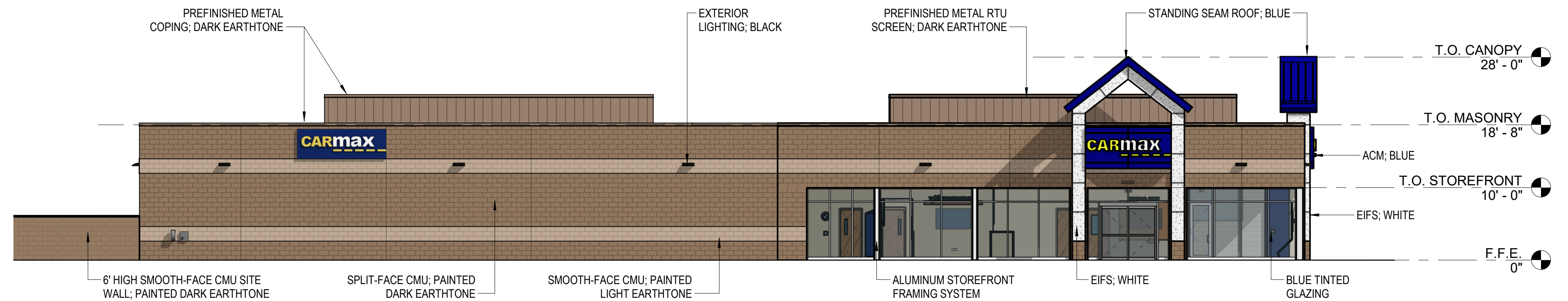
7 SOUTH ELEVATION - CARWASH
A 3.0 3/32" = 1'-0"



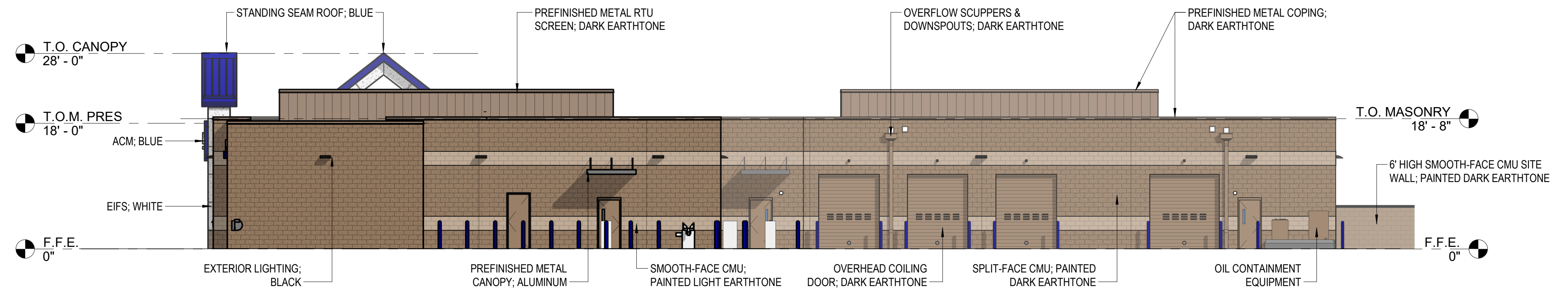
8 EAST ELEVATION - CARWASH
A 3.0 3/32" = 1'-0"



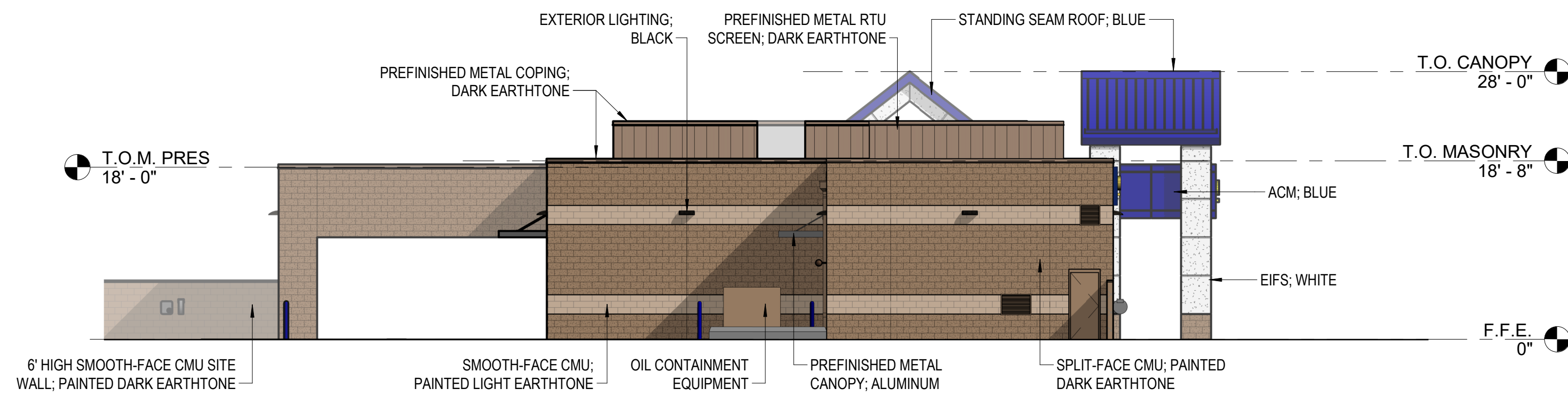
1 EAST ELEVATION
A 3.0 3/32" = 1'-0"



2 SOUTH ELEVATION
A 3.0 3/32" = 1'-0"



3 NORTH ELEVATION
A 3.0 3/32" = 1'-0"



4 WEST ELEVATION
A 3.0 3/32" = 1'-0"





December 2, 2021

Cristobal Carrillo
Community Development Director
City of Visalia
901 E Main Street
Visalia, CA 93292

Subject: Request for Amendment to Visalia Auto Plaza Conditional Zoning Agreement

Dear Mr. Carrillo:

As you know, we have been working with you and other City staff to develop an automobile sales and service facility on the existing 5.91-acre site at West Hillsdale Avenue and North Century Street in the designated Visalia Auto Plaza district. This district is governed by a Conditional Zoning Agreement (CZA 2003-01) that prohibits used car facilities as a standalone use. We have requested an amendment to the agreement that would allow for used car sales if the site is a minimum 5-acre site. The property is currently zoned Service Commercial (C-S) and would remain C-S but with the proposed modifications to the CZA that would allow standalone used car sales.

CarMax expanded into California after the conditional zoning agreement was adopted. The business strategy of CarMax is to provide great customer service and amenities at their used vehicle sales sites. The elevations and floor plan we have provided show that the facility will definitely have the look at feel similar to a new vehicle facility. This type of business strategy may not have been envisioned in 2003, which may have been the reason at the time for excluding standalone used car dealerships. Distinguishing only by whether the vehicles are new or used does not seem as relevant as distinguishing between type, size, and amenities of the facility. We, therefore, request that the City amend the conditional zoning agreement so that CarMax may locate in the Visalia Auto Plaza.

If you have any questions regarding this request, please call John Thatcher at (720) 800-3355, or call me at (559) 259-1466.

Sincerely,

A handwritten signature in blue ink, appearing to read "Steve Brandt".

Steve Brandt, AICP
Principal Planner, QK

cc: John Thatcher, Centerpoint



City of Visalia

Staff Report

Visalia City Council
707 W. Acequia
Visalia, CA 93291

File #: 22-0096

Agenda Date: 3/7/2022

Agenda #: 1.

Agenda Item Wording:

Request for an indefinite continuance of the following item: Public Hearing to consider an appeal of the Planning Commission's approval of Conditional Use Permit No. 2021-28, a request by Caldwell-Mooney Partners II, L.P. to allow a medical use within a portion of an existing 10,200 square foot multi-tenant commercial building. The new medical clinic will be approximately 6,526 square feet in area and will be located within the C-R (Regional Commercial) zone district. The project is located at 3221 South Mooney Boulevard (APN: 121-100-054). Resolution No. 2022-04 required.



City of Visalia

Staff Report

Visalia City Council
707 W. Acequia
Visalia, CA 93291

File #: ****

Agenda Date: 3/7/2022

Agenda #:

Agenda Item Wording:

Upcoming Visalia City Council Meetings:

Monday, March 21, 2022 - Regular Meeting @ 7:00 PM - 707 W. Acequia

Thursday, March 24, 2022 - Special Joint Meeting @ 4:30 PM - 915 S. Mooney Blvd

Monday, April 4, 2022 - Regular Meeting @ 7:00 PM - 707 W. Acequia